

ADMINISTRATIVE ACTION
TYPE 2 CATEGORICAL EXCLUSION

Florida Department of Transportation

SR 524 FROM FRIDAY ROAD TO INDUSTRY ROAD

District: FDOT District 5

County: Brevard County

ETDM Number: 14321

Financial Management Number: 437983-1-21-01

Federal-Aid Project Number: D518-034-B

Project Manager: Maria Serrano-Acosta

The Environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. § 327 and a Memorandum of Understanding dated May 26, 2022, and executed by the Federal Highway Administration and FDOT.

This action has been determined to be a Categorical Exclusion, which meets the definition contained in 23 CFR 771.115(b), and based on past experience with similar actions and supported by this analysis, does not involve significant environmental impacts.

Signature below constitutes Location and Design Concept Acceptance:

A handwritten signature in blue ink, appearing to read 'K. Serrano', is written over a solid black horizontal line.

May 29, 2026

Director Office of Environmental Management
Florida Department of Transportation

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This document was prepared in accordance with the FDOT PD&E Manual.

This project has been developed without regard to race, color or national origin, age, sex, religion, disability or family status (Title VI of the Civil Rights Act of 1964, as amended).

On **06/20/2019** the State of Florida determined that this project is consistent with the Florida Coastal Zone Management Program.

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1. Project Information

1.1 Project Description

This project involves the two-lane to four-lane widening of the approximately 3.15 mile segment of State Road (SR) 524 from Friday Road (south) to Industry Road in the City of Cocoa in Brevard County, in addition to the modifications of the existing interchange at Interstate 95 (I-95) and SR 524 (see location map in **Figure 1-1**).

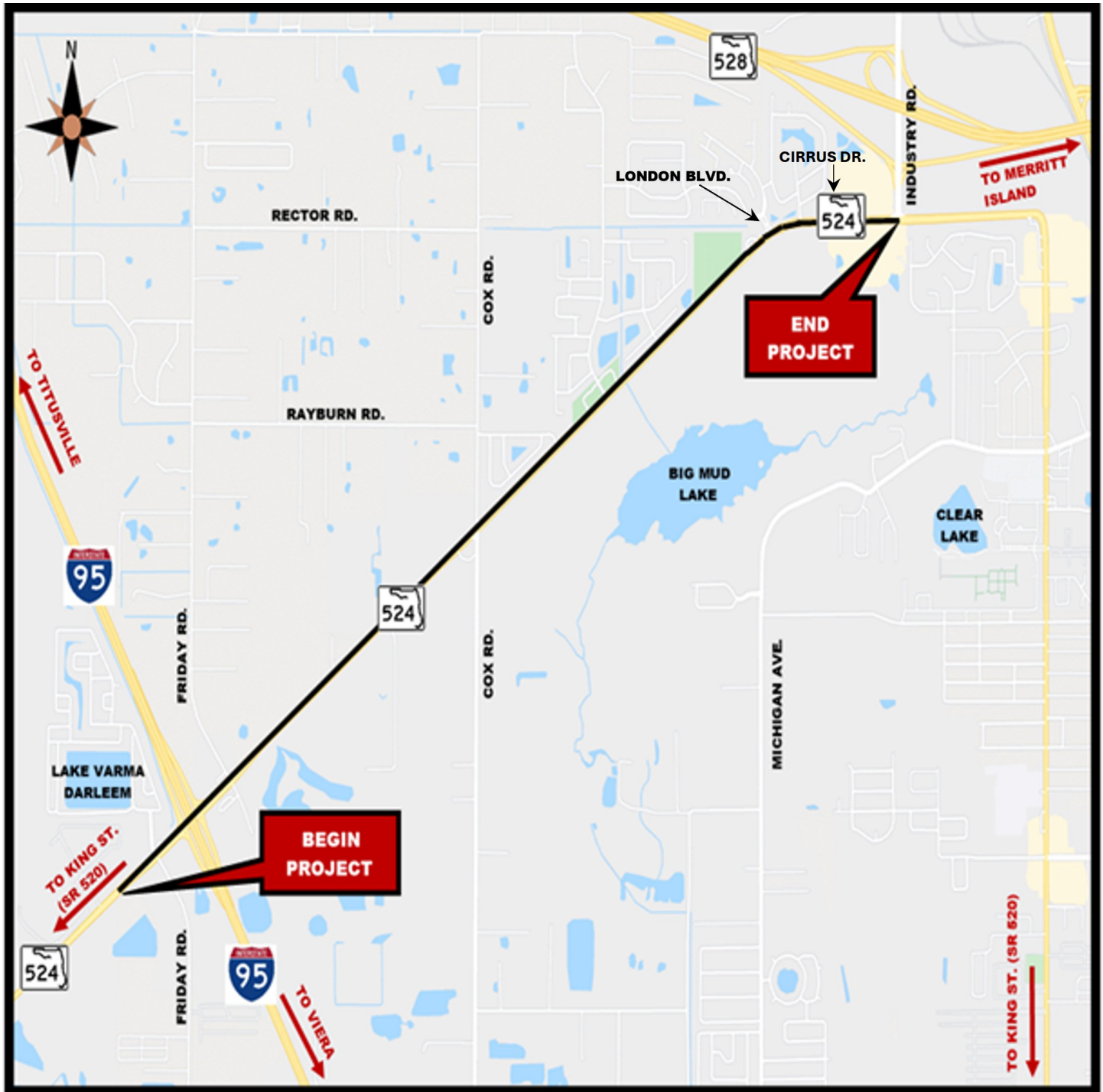


Figure 1-1: Project Location Map

Existing Conditions

Within the project limits, SR 524 exists as a two-lane urban minor arterial comprised of one 12-foot lane in each direction with 10-foot shoulders (4-foot paved). Intermittent sidewalks are located along the north side of SR 524 between Cox Road and Industry Road and along the south side from the Cirrus Drive intersection to Industry Road, while the existing paved shoulders serve as undesignated bike lanes. The existing roadway corridor is offset from the center of a typical 200-foot right-of-way (ROW). The ROW varies through the horizontal curve located near the London Boulevard intersection and widens to 230 foot to the intersection at Industry Road. The SR 524 corridor contains an interchange with I-95 between Friday Road (south) and Friday Road (north) and eight signalized intersections. Stormwater flows off the roadway into roadside ditches. Specific typical sections along SR 524 are described in greater detail below.

Friday Road (south) to I-95

SR 524 has one 12-foot lane in each direction separated by a 40-foot grassed median. A westbound left-turn lane is located at the Friday Road (south) intersection, while eastbound and westbound auxiliary lanes are present to and from southbound I-95. See **Figure 1-2** for the typical section.

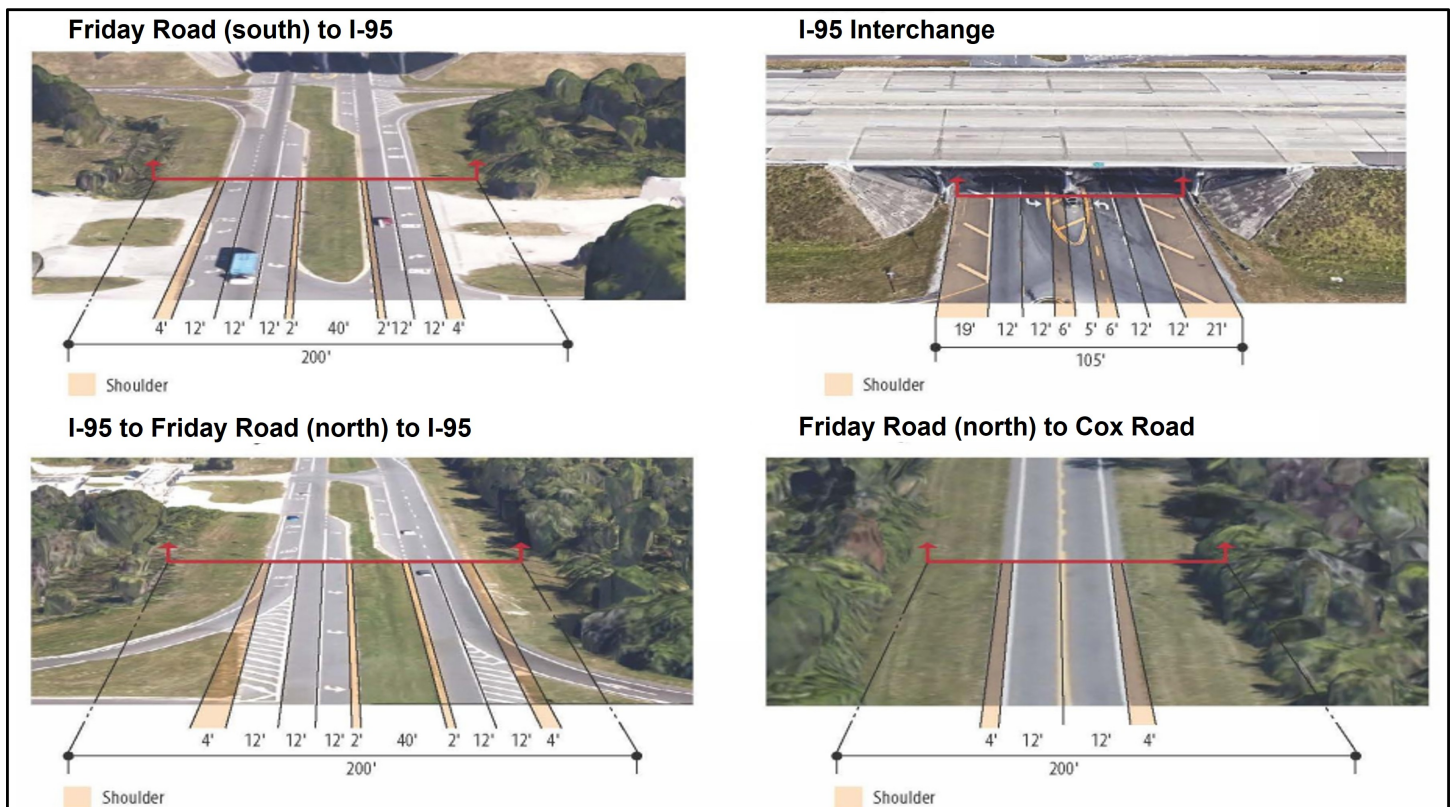


Figure 1-2: Existing SR 524 Typical Sections from Friday Road (south) to Cox Road

I-95 Interchange

Beneath the I-95 overpass, SR 524 has one lane in each direction with 12-foot left-turn lanes in the median to access I-95 entrance ramps. The travel lanes are bordered with 19 and 21-foot wide outside shoulders. See **Figure 1-2** for the typical section.

I-95 to Friday Road (north)

This section of SR 524 has one 12-foot lane in each direction separated by a 40-foot grassed median. An eastbound left-turn lane is located at the Friday Road (north) intersection, while eastbound and westbound auxiliary lanes are present to and from northbound I-95. See **Figure 1-2** for the typical section.

Friday Road (north) to Cox Road

This section of SR 524 exists as a two-lane undivided roadway with one 12-foot lane and 4-foot paved shoulders in each direction. Roadside ditches capture stormwater runoff and convey it to outfall locations. The roadway typical section widens to include a westbound left-turn lane and an eastbound right-turn lane into the signalized intersection at the Walmart Distribution Center. See **Figure 1-2** for the typical section.

Cox Road to London Boulevard

The existing roadway consists of an undivided two-lane roadway with one 12-foot lane and 4-foot paved shoulders in each direction and roadside ditches to capture stormwater runoff. An existing sidewalk, approximately 6 feet in width, is situated near the north ROW line. The roadway typical section widens at Cox Road (signalized), Pinyon Drive, Westminster Drive, Lance Boulevard, and London Boulevard (signalized) to allow for left and right-turn lanes. See **Figure 1-3** for the typical section.

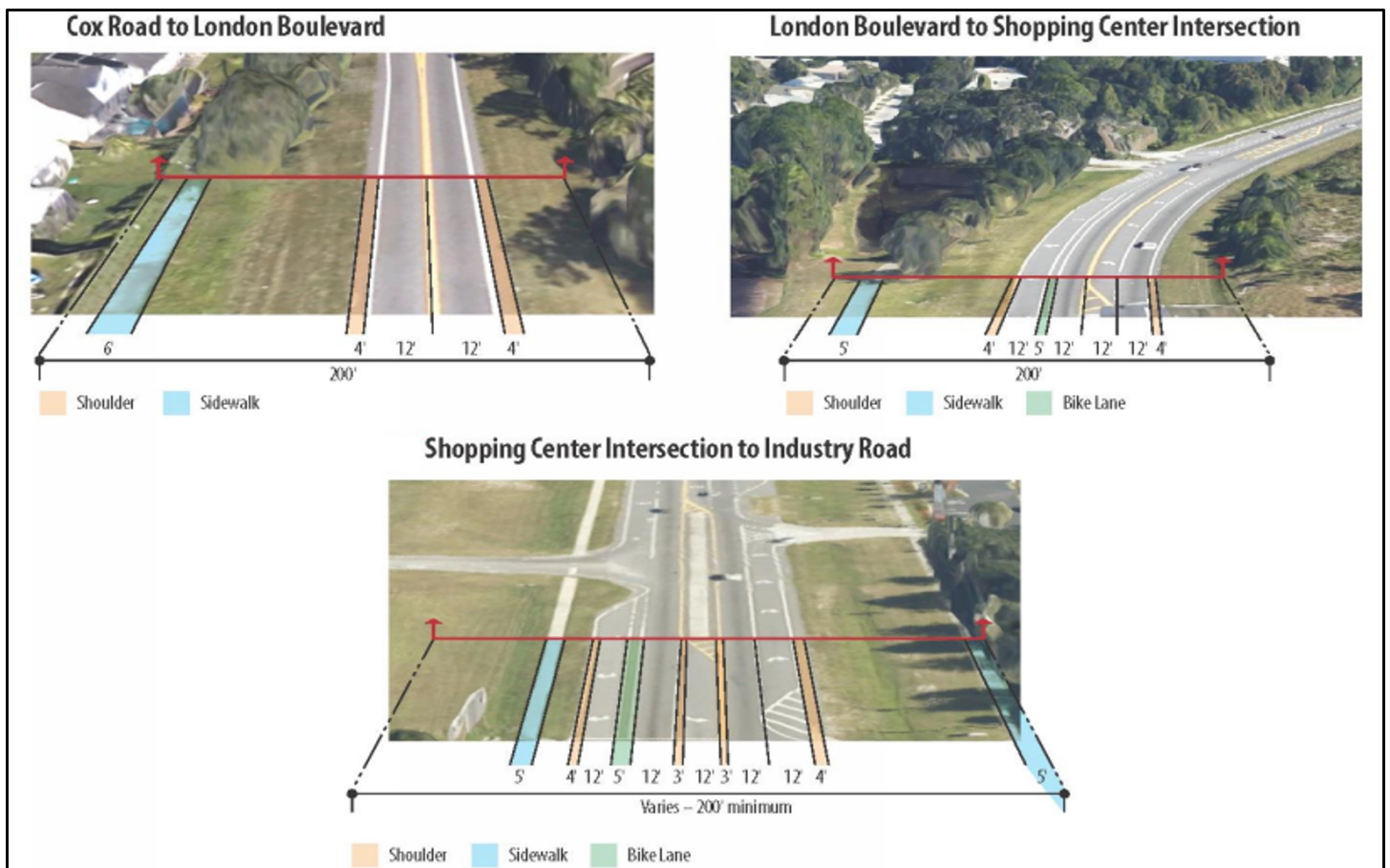


Figure 1-3: Existing SR 524 Typical Sections from Cox Road to Industry Road

London Boulevard to Cirrus Drive/Shopping Center Intersection

Between London Boulevard and Cirrus Drive/Shopping Center Intersection, SR 524 exists as a two-lane roadway with one 12-foot lane and 4-foot paved shoulder in each direction, as well as a 5-foot westbound bike lane. An existing sidewalk, approximately 5 feet in width, is situated near the north ROW line. There is a continuous left-turn lane for the intersections of London Boulevard, Coventry Court, and Cirrus Drive access road. A westbound right-turn lane is provided at London Boulevard and Coventry Court, while an eastbound right-turn lane is provided at the CVS signalized intersection. See **Figure 1-3** for the typical section.

Cirrus Drive/Shopping Center Intersection to Industry Road

This 1,100-foot section consists of a two-lane divided roadway with one 12-foot lane and 4-foot outside shoulder in each direction, as well as a 5-foot westbound bike lane. Sidewalks, approximately 5 feet in width, are situated on both sides of the roadway. The east approach at the signalized Cirrus Drive has left and right-turn lanes in the west direction, while the west approach at the signalized intersection at Industry Road has eastbound left and right-turn lanes. See **Figure 1-3** for the typical section.

Interstate 95 (I-95)

The I-95 typical section consists of a six-lane divided concrete roadway with three 12-foot lanes and 12-foot inside and outside shoulders in each direction separated by a concrete barrier wall. Northbound and southbound bridges (Bridge Numbers 700128 and 700054, respectively) span the SR 524 roadway corridor with an existing vertical clearance of 16.04-feet. The outside shoulders along the bridges are approximately 15-foot wide. The I-95 typical section lies within 300-feet of limited-access ROW. The limited-access ROW expands to 500-feet where the SR 524 on and off-ramps are introduced. See **Figure 1-4** for the typical section.

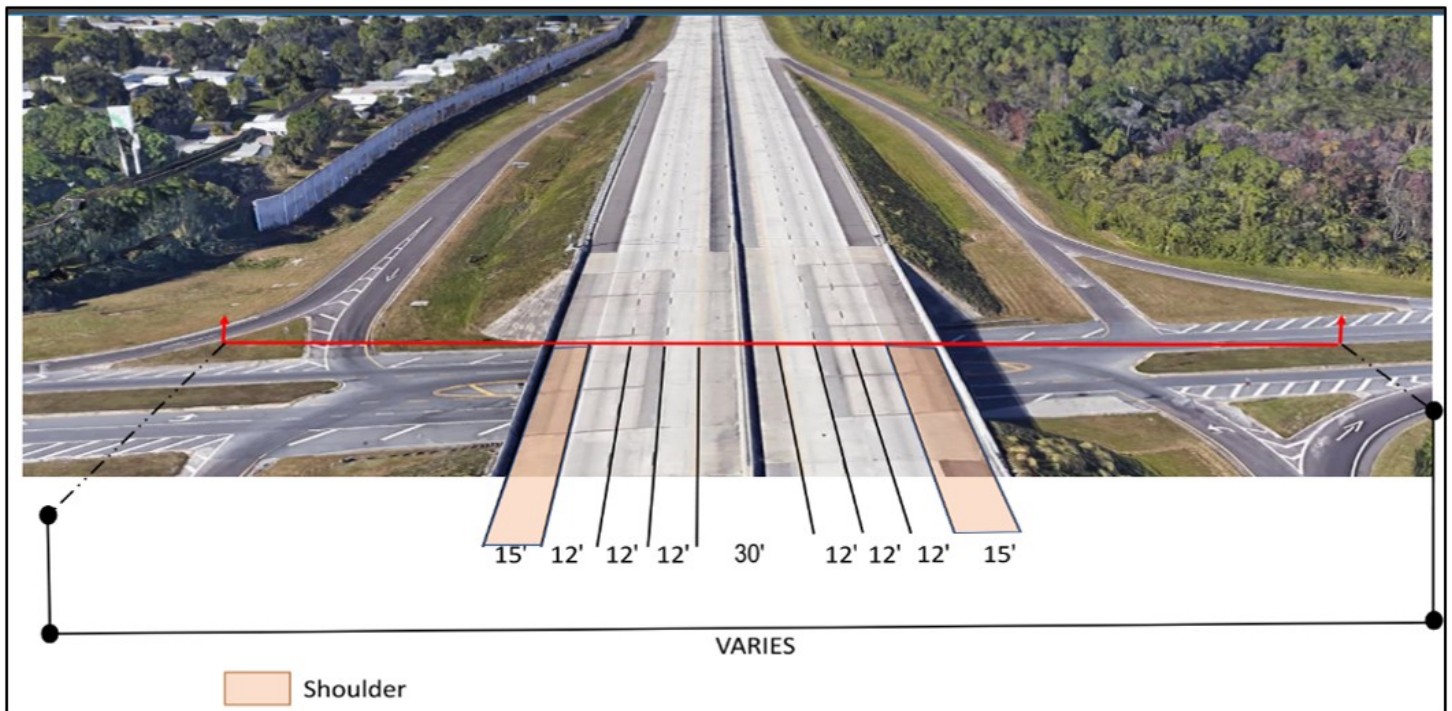


Figure 1-4: Existing I-95 Typical Section

Preferred Alternative

The Preferred Alternative includes widening SR 524 to a four-lane divided urban arterial generally with a 22-foot wide median. The lane width will include a combination of 11 and 12-foot wide lanes throughout the corridor. Curb and gutter

with a 14-ft wide shared use path will be included on both sides of the roadway. Intersection improvements include converting the existing I-95 interchange to a Diverging Diamond Interchange (DDI), roundabouts at Cox Road and London Boulevard and signalized intersections at Friday Road (south), Friday Road (north), Walmart Distribution, Cirrus Drive, and Industry Road. The proposed DDI at the I-95 interchange will require replacement of the I-95 bridges over SR 524. Three noise barriers are proposed to mitigate noise impacts along the corridor. The proposed improvements include construction of two new stormwater off-site ponds and two ponds within existing FDOT ROW.

The project corridor was separated into four segments as depicted in **Figure 1-5**, which also shows the locations of the preferred pond sites 1A, 2F, 3A and 3B.

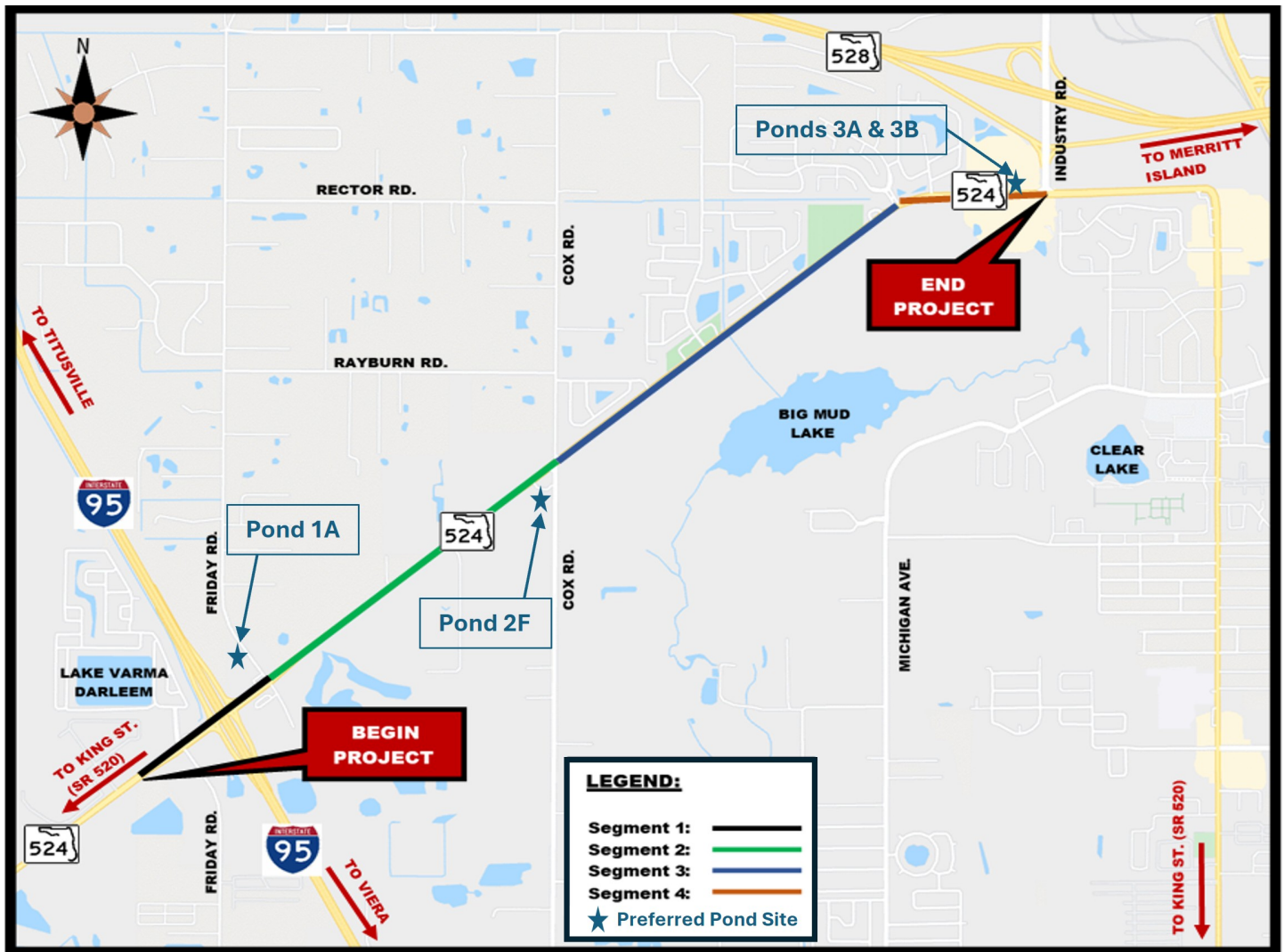


Figure 1-5: Project Segment and Preferred Pond Sites Map

The segments are labeled numerically from west to east, separated by intersections. Segment 1 starts at the beginning of the project at the Friday Road (south) intersection and goes through the I-95 interchange, ending at the Friday Road (north) intersection. Segment 2 starts at the Friday Road (north) intersection and ends at the Cox Road intersection. Segment 3 starts at the Cox Road intersection and ends at the London Boulevard intersection. Segment 4 starts at the London Boulevard intersection and ends at the end of the project, Industry Road.

The preferred alternative for each segment is identified below based on engineering and environmental factors and public and agency input.

Segment 1

Segment 1 will be a four-lane divided section that runs between Friday Road (south) and Friday Road (north) with a DDI at I-95. The typical section outside the DDI has 12-foot travel lanes, Type F outside curb and gutter, Type E inside curb and gutter, 14-foot shared-use paths on either side of the corridor, and a varying median (22 - 80 feet). The travel lanes widen to 14-foot within the DDI limits and include an additional 14-foot left-turn lane. The I-95 overhead bridge will be replaced. This typical section is shown in **Figure 1-6**.

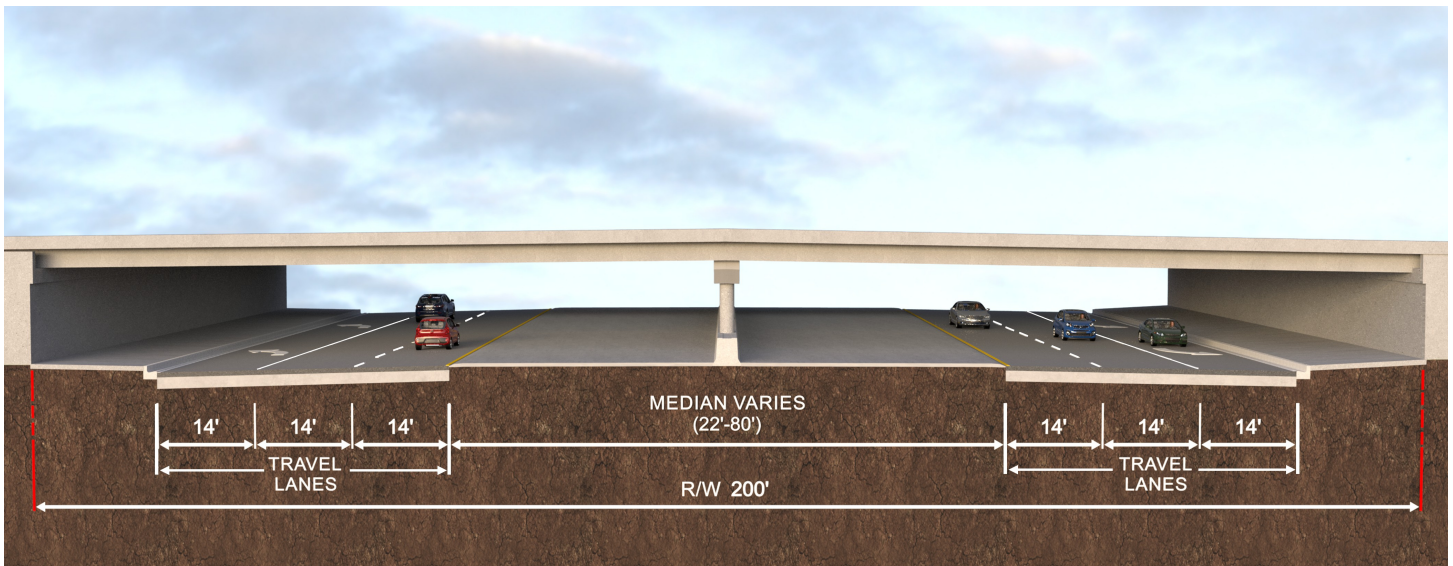


Figure 1-6: Preferred Alternative Typical Section Segment 1

Segment 2

Segment 2 will be a four-lane divided section that runs from Friday Road (north) to Cox Road. This section has Type F outside curb and gutter, 12-foot outside lanes, 11-foot inside lanes, Type E inside curb and gutter, and a 22-foot median. Each side of the corridor has 14-foot shared-use paths 2-foot from the existing ROW. Drainage swales with 1:4 front and back slopes will be placed between the shared-use path and the outside curb and gutter. This typical section is shown in **Figure 1-7**.

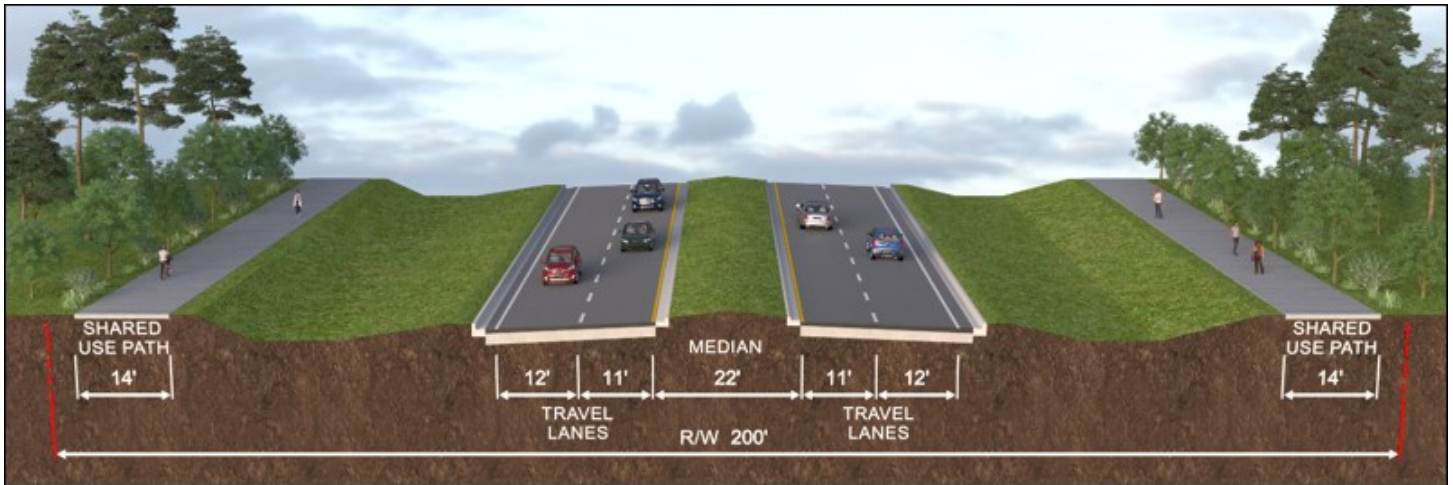


Figure 1-7: Preferred Alternative Typical Section Segment 2

Segment 3

Segment 3 will be a four-lane divided section that runs from Cox Road to London Boulevard. This section has Type F outside curb and gutter, 11-foot travel lanes, Type E inside curb and gutter, and a varying median (22 - 60 feet). Each side of the corridor has 14-foot shared-use paths 2-foot from the existing ROW. Drainage swales with 1:4 front and back slopes will be placed between the shared-use path and the outside curb and gutter. This typical section is shown in **Figure 1-8**.

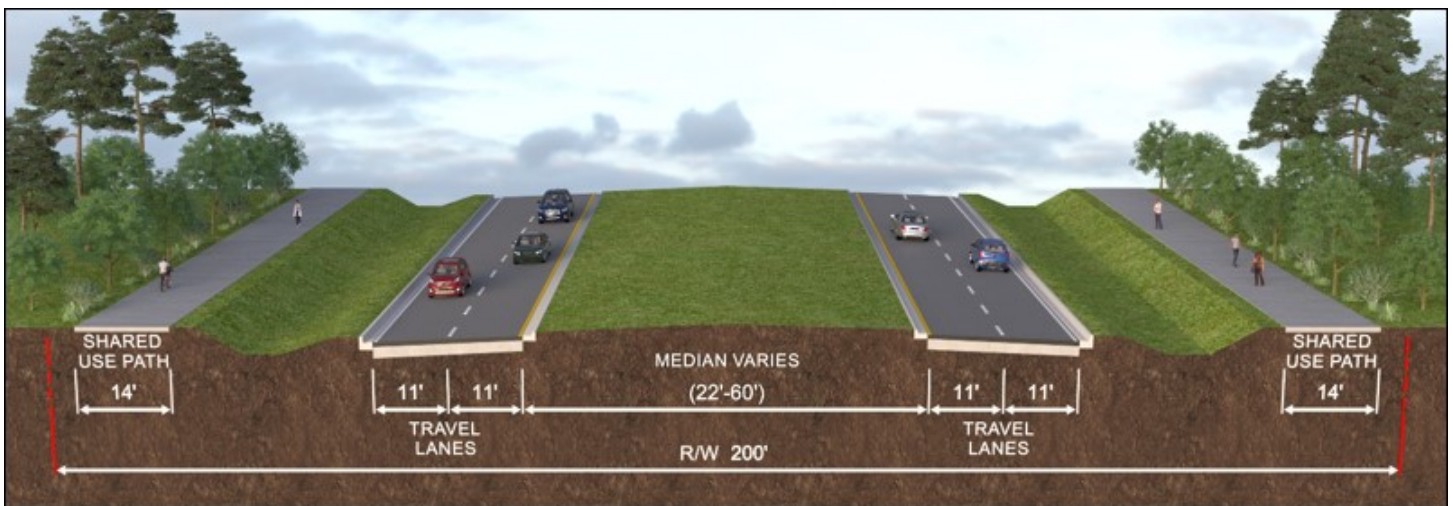


Figure 1-8: Preferred Alternative Typical Section Segment 3

Segment 4

Segment 4 will be a four-lane divided section that runs from London Boulevard to Industry Blvd. This section has Type F outside curb and gutter, 11-foot travel lanes, Type E inside curb and gutter, and a 22-foot median. Each side of the corridor has 14-foot shared-use paths. The existing ROW widens on the north side, but the horizontal alignment will be at the same offset from the centerline as in segments 2 and 3 (50-foot). Drainage swales with 1:4 front and back slopes will be placed between the shared-use path and the outside curb and gutter except where the shared-use path comes in closer to the road at a cross drain at station 527+00 and to tie back into the existing curb ramp configuration at Industry Road. This typical section is shown in **Figure 1-9**.

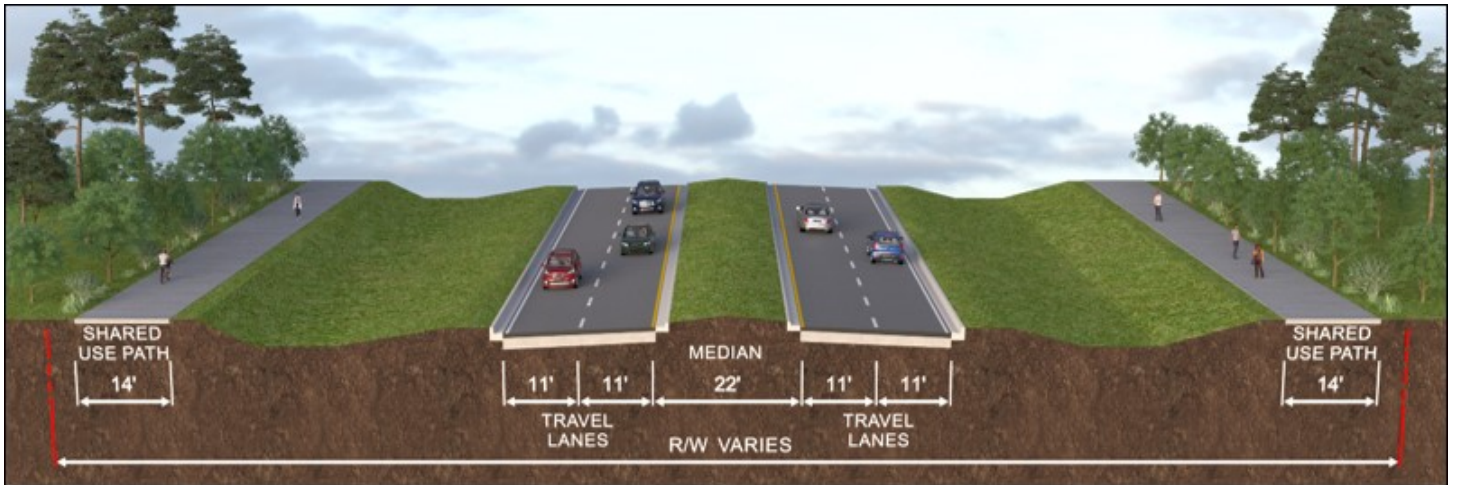


Figure 1-9: Preferred Alternative Typical Section Segment 4

Preferred intersection improvements include a DDI at I-95 (**Figure 1-10**), roundabouts at Cox Road (**Figure 1-11**) and London Boulevard (**Figure 1-12**), signaling the intersection at Friday Road (south), and the signalized intersections will continue to be signalized at Friday Road (north), Walmart Distribution, Cirrus Drive, and Industry Road.

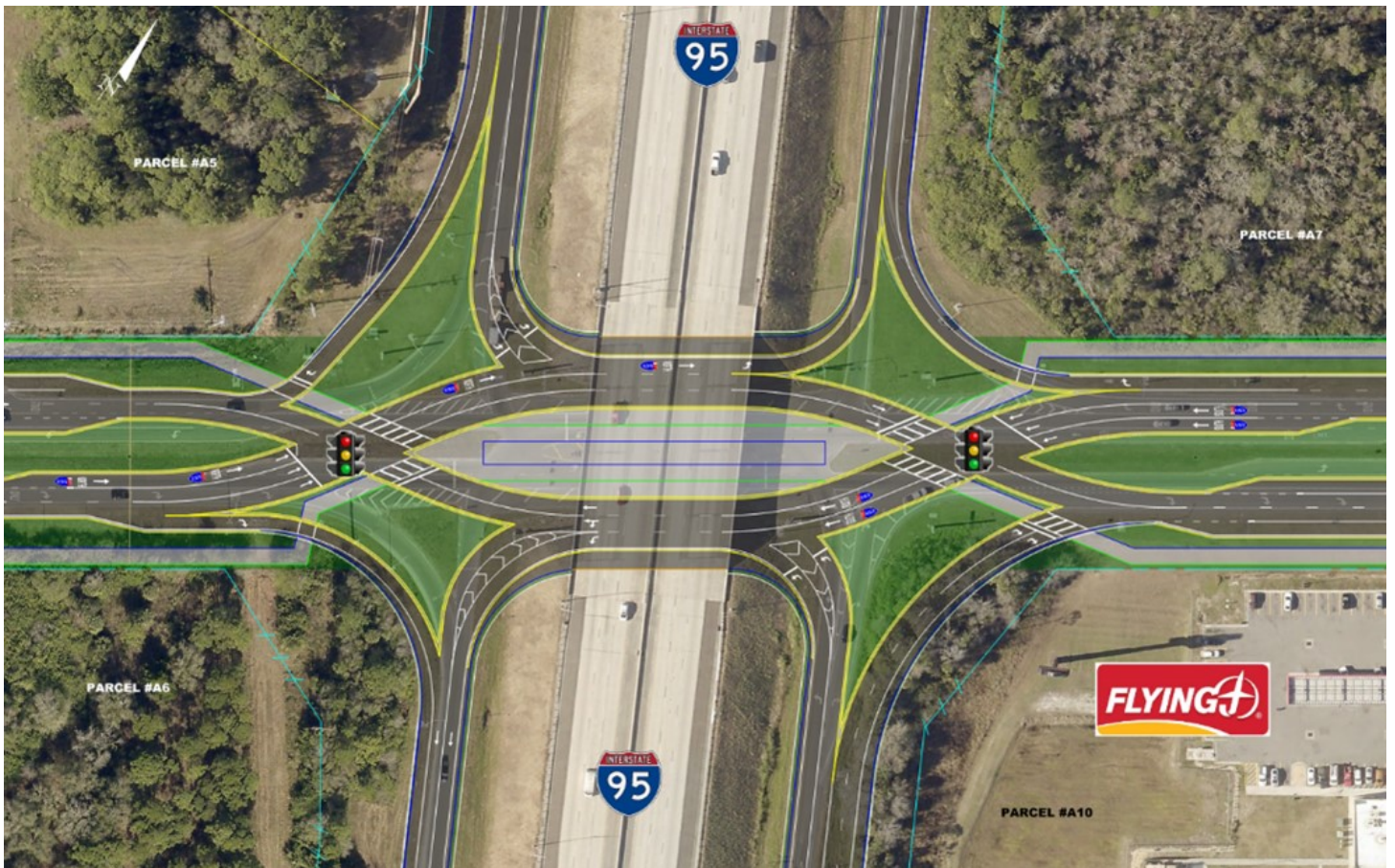


Figure 1-10: Preferred Alternative I-95 Diverging Diamond Interchange



Figure 1-11: Preferred Alternative Roundabout at Cox Road



Figure 1-12: Preferred Alternative Roundabout at London Boulevard

The Preferred Alternative also includes four proposed stormwater pond sites: 1A, 2F, 3A and 3B. Sites 1A and 2F will require the acquisition of additional ROW, while sites 3A and 3B are within existing FDOT ROW. Full or partial ROW acquisition of 13 parcels (approximately 8.2 acres) are anticipated for the roadway widening, roundabouts and the pond sites.

The conceptual plans for the Preferred Alternative are shown in Appendix C of the Preliminary Engineering Report (PER) which is available in the project file.

1.2 Purpose and Need

Purpose

The purpose of this project is to improve roadway capacity to accommodate year 2045 future travel demand and emergency response, improve traffic flow and improve safety for vehicles, bicycles and pedestrians.

Need

The need for the project is based on transportation demand/capacity, emergency response, and safety.

Project Status

The project lies within the jurisdictions of the Space Coast Transportation Planning Organization (SCTPO), Brevard County, and the city of Cocoa. The SCTPO governing board adopted the 2050 Long Range Transportation Plan (LRTP)

on September 11, 2025. The Preliminary Engineering (Design Phase) was funded in previous 2045 LRTP. ROW and Construction are funded and shown in the middle years of the LRTP (FY 2031-2040). The project is listed in the Fiscal Year (FY) 2024/25-2028/29 SCTPO Transportation Improvement Plan (TIP) with Preliminary Engineering (design) phase fully funded in FY 2025. The Preliminary Engineering (design) phase is fully funded in the FY 2026-2030 Work Program and State Transportation Improvement Plan (STIP) in prior year FY 25 and FY 2026.

Roadway Capacity

Along SR 524 within the project limits, the existing Annual Average Daily Traffic (AADT) ranges from 9,400 to 17,000 vehicles per day (vpd) in 2022 and is expected to grow to a range of 16,000 to 27,000 vpd by year 2045. The following segments are projected to operate below the target level of service (LOS) D within future No-Build conditions.

Year 2035: Eastbound SR 524 between I-95 northbound (NB) Ramps and Friday Road (north), eastbound SR 524 between Cirrus Drive and Industry Road, and westbound SR 524 between Friday Road (south) and I-95 Southbound (SB) ramps.

Year 2045:

AM - Eastbound SR 524 between I-95 NB Ramps and Friday Road (north), between Walmart and Cox Road, and between London Boulevard and Industry Road. Westbound SR 524 between the Cirrus Drive and Industry Road.
 PM - Eastbound and westbound SR 524 between I-95 SB Ramps and Walmart and between Cirrus Drive and Industry Road.

Additionally, the Florida Division of Emergency Management's State Emergency Response Team (SERT) maps for Brevard County identify multiple areas within and surrounding the project limits, including Merritt Island, Cocoa Beach, and Cape Canaveral, designated as hurricane evacuation Zone A. The designated evacuation routes for this area include I-95, SR 528, SR 520, US 1, and SR 524. Constructed in 2018, Fire Station No. 3 is located along the southeast side of SR 524, just west of the London Boulevard intersection.

Safety

Five years of crash data (from January 1, 2019, to December 31, 2023) along SR 524 were obtained from the Signal Four Analytics (S4) and supplemented with data from the FDOT Crash Analysis Reporting System (CARS). A total of 297 crashes occurred within the study limits, with approximately 60 crashes per year. Nearly 82% (243) of the crashes were located at the seven signalized intersections and more than 31% of crashes at intersections resulted in injuries. The most common crash types involved rear ends (31.0%), left-turns (20.2%), and head on (18.2%). A total of 54 crashes within the 5 year period were head-on crashes that typically involve more severe results. The existing two-lane undivided roadway plays a major factor in this type of crash. Overall, the total number of crashes accounted for three fatalities and 164 injuries. Within the study limits, the calculated crash rate in millions vehicle miles traveled is 3.71, more than 185% higher than the statewide average of 1.29 for a similar roadway type.

1.3 Planning Consistency

The relevant pages from the LRTP and TIP from the SCTPO and the STIP are attached.

Currently Adopted LRTP-CFP	COMMENTS
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Yes	Space Coast TPO 2050 LRTP adopted September 11, 2025, Table 4-3 (LRTP page 129), Project Number 2 PE funded in prior year \$7,402,342 ROW funded \$9,242,230 in FY 2031-2035 and \$2,219,400 in FY 2036-2040 (\$11,461,630 total in year of expenditure) CST funded \$11,466,671 in FY 2036-2040 (year of expenditure)			
	Currently Approved	\$	FY	COMMENTS
PE (Final Design)				
TIP	Y	7,402,342	2025	Project listed in Amended 2025-2029 TIP - Page 64 as of 12/17/2024. Project not listed in current 2026-30 TIP as PE was fully funded in prior TIP
STIP	Y	3,131,065 4,295,427	2025 2026	Adopted STIP as of 2/6/2025. FY 25 - \$3,131,065 and FY 26 \$4,295,427 Total PE funding \$7,426,492
R/W				
TIP	N			
STIP	N			
Construction				
TIP	N			
STIP	N			

2. Environmental Analysis Summary

Issues/Resources	Significant Impacts?*			
	Yes	No	Enhance	NoInv
3. Community Effects				
1. Social	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Economic	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Land Use Changes	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. Mobility	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
5. Aesthetic Effects	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
6. Relocation Potential	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
7. Farmland Resources	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. Cultural Resources				
1. Section 106 of the National Historic Preservation Act	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Section 4(f) of the USDOT Act of 1966, as amended	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Section 6(f) of the Land and Water Conservation Fund	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. Recreational Areas and Protected Lands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
5. Natural Resources				
1. Protected Species and Habitat	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Wetlands and Other Surface Waters	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Essential Fish Habitat (EFH)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. Floodplains	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. Sole Source Aquifer	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. Water Resources	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
7. Aquatic Preserves	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
8. Outstanding Florida Waters	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
9. Wild and Scenic Rivers	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
10. Coastal Barrier Resources	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. Physical Resources				
1. Highway Traffic Noise	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
2. Air Quality	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
3. Contamination	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
4. Utilities and Railroads	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5. Construction	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

USCG Permit

- A USCG Permit IS NOT required.
- A USCG Permit IS required.

* **Impact Determination:** Yes = Significant; No = No Significant Impact; Enhance = Enhancement; NoInv = Issue absent, no involvement. Basis of decision is documented in the following sections.

3. Community Effects

The project will not have significant community impacts. Below is a summary of the evaluation performed.

3.1 Social

The study area, located in the incorporated City of Cocoa in Brevard County, is mostly developed properties consisting of commercial with scattered residential and several areas of undeveloped land. The Sociocultural Data Report (SDR, February 2025), included in the project file, identified demographics within the 500-foot project buffer and within Brevard County from the 2020 census and 2019-23 American Community Survey (ACS). The ACS data identified the study area's population is 763 people residing in 305 households. **Table 3-1** provides a comparison of demographic data between the project area and Brevard County.

Demographic Item	Project Area	Brevard County
Total Population	763	620,533
Minority Population (Race and Ethnicity)	30.28%	28.74%
Median Age (years)	50	47
Population Under Age 5	4.85%	4.41%
Population Age 65 and Over	23.85%	24.20%
Median Household Income	\$48,765	\$75,817
Population Below Poverty Level	12.19%	9.90%
Households with Public Assistance Income	1.31%	2.10%
Population Ages 20-64 with a Disability	10.50%	12.61%
Population Speaks English Less than Well	0.55%	3.53%
Owner-Occupied Units	75.07%	65.80%
Occupied Units with No Vehicle	0.65%	4.61%
Source: SDR, ACS 2019-23		

Table 3-1: Demographic Comparison - Project Area vs. Brevard County

The minority population with 231 people, makes up 30.28% of the total population comprising "Black or African American Alone" with 89 people (11.66%) "Hispanic or Latino of Any Race (Ethnicity)" with 112 people (14.68%), "American Indian or Alaska Native Alone" with 10 people (1.31%), and "Claimed 2 or More Races" with 100 people (13.11%). The minority population is slightly higher in the project area than in Brevard County (28.74%).

The percent of the total population within Brevard County who reported that they speak English "less than very well" is 3.53% and within the project area is 0.55% based on the 2019-23 ACS. This includes people who speak Spanish, Indo-European languages, Asian and Pacific Island languages, and "Other" languages.

There are three census blocks with a minority population greater than 40% within the 500-ft. buffer of the project. In the Preferred Alternative, there are thirteen parcels (partial acquisition of nine parcels for intersection improvements and four parcels for pond sites) which could be impacted, none of which require residential relocation. There is one potential business relocation (gas station) that has been identified for one of the preferred pond sites and is documented in the Conceptual Stage Relocation Plan (CSR-October 2024), available in the project file. A majority of the comments received at the Alternatives Public Meeting were in support of the project.

The Preferred Alternative will result in minimal impacts to properties, property access and relocations. Therefore, the Preferred Alternative does not cause disproportionate impacts to distinct communities.

There are no barriers, retaining walls, or elevated structures in the Preferred Alternative that would impact, divide, or result in decreased connections to communities or activity centers. The Preferred Alternative's proposed shared use path on both sides of SR 524 will maintain community cohesion by allowing bicyclists and pedestrians safe access along SR 524 by filling gaps in existing sidewalks. The City of Cocoa Fire Department's Station 3 is located along the south side of SR 524 just west of London Boulevard. To provide emergency access to and from the fire station, a traversable median with mountable curb is proposed at their entrances. The proposed improvements will also maintain access for Emergency Services to respond, as needed, in a timely manner. Emergency response times are anticipated to be reduced as a result of the added capacity along the project corridor.

No direct impacts to community centers are anticipated from the proposed improvements. Temporary, indirect impacts from construction activities may occur; however, no adverse impacts are anticipated.

3.2 Economic

The relocation of one business as documented in the CSRP, prepared October 2024 and in the project file, is anticipated with the proposed improvements for constructing a pond site through the acquisition of a parcel that is the current site of a gas station. The parcel is owned by a landlord and located along the south side of SR 524.

Aside from one business relocation, there are no changes to business access. The proposed project has the potential to generate additional employment opportunities by enhancing connectivity to local and regional employers through improved safety and LOS. The addition of multi-modal accommodations in the form of shared use paths on both sides of the roadway throughout the project limits will also provide improved access to businesses for non-motorized users within the study area.

3.3 Land Use Changes

The existing land use map for the portion of Brevard County that encompasses the project area is attached as **Figure 3-1**. Florida Land Use Cover and Forms Classification System (FLUCCS) data, and aerial photographs were utilized to determine current land use within the corridor. The future land use maps for the City of Cocoa and Brevard County are attached as **Figure 3-2** and **Figure 3-3** respectively. The land uses within the project corridor were subsequently verified during field visits in April 2019 and February 2020. For evaluating land use within the study area, a 500-ft buffer was created from the existing centerline of SR 524 from Friday Road (south) to Industry Road. The predominant land uses within the 500-ft buffer of the study area are vacant non-residential, acreage not zoned for agriculture, residential and retail/office, as shown on **Table 3-2**.

Description	Acres	Percentage
Acreage not zoned for Agriculture	60.94	21.11%
Agricultural	0.00	0.00%
Industrial	0.40	0.14%
Institutional	2.10	0.73%
Parcels with No Values	1.39	0.48%
Public/Semi-Public	9.06	3.14%
Residential	56.82	19.68%
Retail/Office	34.01	11.78%
Vacant Non Residential	116.97	40.50%
Vacant Residential	7.05	2.44%

Totals	288.74	100.00%
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Table 3-2: Existing Land Use

The Preferred Alternative will require additional ROW at the Cox Road and London Boulevard intersections for construction of proposed roundabouts and at two off-site pond sites (1A and 2F). The project is consistent with the Brevard County future land use map and will not change the existing land use patterns. The project will not induce secondary development or change existing land use patterns. The roadway improvements are anticipated to accommodate increased travel demand from population and employment growth of the area.

The proposed project is compatible with the 1988 Brevard County Comprehensive Plan, updated February 2022 and the City of Cocoa Comprehensive Plan 2020-2030 adopted February 26, 2020, and is included in the SCTPO's 2045 LRTP.

3.4 Mobility

As documented in the Project Traffic Analysis Report (PTAR-July 2019) and the PTAR Reevaluation Memo (April 2024) located in the project file, the project will enhance mobility along SR 524 as well as at the I-95 interchange ramp terminals and at intersection of Industry Road which connects to both SR 528 to the north and SR 501 to the east through reducing congestion and improving traffic operations. The proposed capacity improvements will improve vehicular mobility along the corridor. The addition of 14-foot shared-use paths on each side of SR 524 will enhance bicycle and pedestrian connectivity. Capacity improvements along SR 524 will enhance emergency evacuation, and provide improved emergency response within the study area.

SR 524 is important to local and regional transportation as it provides access between I-95 and SR 528 in Brevard County, and it is utilized to connect to hurricane evacuation routes. SR 524 is essential in the movement of residents and goods within the study area. The Space Coast Area Transit (SCAT) operates two bus routes (6 and 11) which serve the project's east limits at Industry Road. There is a transfer point at Cocoa Commons for these two routes. SCAT does not operate any bus stops along SR 524, therefore the Preferred Alternative will not impact access to public transit.

SR 524 is an existing undivided roadway and existing side streets are provided full access. The Preferred Alternative includes widening the roadway to 4-lane roadway and installation of a raised divided median along with intersection improvements. A DDI is proposed at the I-95 interchange to optimize traffic operations and enhance safety through the interchange, including the ramp terminals. Roundabouts are proposed at the Cox Road and London Boulevard intersections to facilitate u-turn movements and traffic flows from side streets. Full and directional median openings are proposed at other existing unsignalized intersections along SR 524 as shown in the Concept Plans in the PER located in the project file.

The Preferred Alternative will enhance mobility in the study area.

3.5 Aesthetic Effects

The viewshed within the study area is not expected to change since the proposed improvements are located within or adjacent to the existing ROW. The proposed off-site ponds will not alter aesthetics. There are no historic resources that are identified as eligible for listing in the National Register of Historic Places (NRHP) that would experience viewshed impacts. Median landscaping is included in the Preferred Alternative to enhance the aesthetics of the corridor and provide traffic calming measures along the corridor, specifically between Cox Road and London Boulevard. In addition to the

landscaping, proposed roundabouts at Cox Road and London Boulevard will enhance the overall aesthetics and viewshed.

3.6 Relocation Potential

The roadway improvements in the Preferred Alternative can be built within the existing ROW for most of the project except at the proposed roundabouts at Cox Road and London Boulevard. Partial acquisition of nine parcels (0.38 acres of ROW) will be required for the roadway improvements. The Concept Plans of the Preferred Alternative, in Appendix C of the PER (May 2026) in the project file, show the properties where ROW acquisition is proposed.

Two preferred pond sites will require acquisition of 7.86 acres of ROW from 4 parcels. No residential relocations are required. One business relocation, a gas station at the intersection of SR 524/Cox Road, is required for preferred pond site 2F. A CSRP (October 2024) was prepared and is included in the project file. There are an adequate number of commercial properties (1 gas station) and vacant lands for sale (9) as potential replacement sites within 10 miles of the project corridor.

In order to minimize the unavoidable effects of Right of Way acquisition and displacement of people, a Right of Way and Relocation Assistance Program will be carried out in accordance with Section 421.55, Florida Statutes, Relocation of displaced persons, and the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970 (Public Law 91-646 as amended by Public Law 100-17).

3.7 Farmland Resources

This project is within the Palm Bay-Melbourne Urbanized Area, the geographic information system (GIS) analysis data indicates that some of this land is undeveloped and therefore, may be subject to the Farmland Protection Policy Act (FPPA). The Preferred Alternative includes 2 off-site pond sites 1A and 2F which will require the acquisition of 7.86 acres of additional ROW.

Additional coordination was conducted with US Department of Agriculture Natural Resources Conservation Service (NRCS) to verify that no adverse effects to Farmland of Unique Importance will occur. A farmland involvement determination was submitted to the NRCS in December 2024 that included a map of the Preferred Alternative and pond sites, Farmland Conversion Impact Rating for Corridor Type Projects (NRCS-CPA-106) with Parts I and III completed and GIS shapefiles for the Preferred Alternative. NRCS responded on December 16, 2024 updating the NRCS-CPA-106 form. There are approximately 48 acres of prime and unique farmland within the study area, with 0 acres to be converted as a result of the Preferred Alternative. The transmittal to NRCS and response from NRCS are included in the project file and form NRCS-CPA-106 (December 2024) is attached. The project results in an impact rating of 52.4 out of 260 points, which is below the 160-point threshold for additional analysis and no further coordination is required.

4. Cultural Resources

The project will not have significant impacts to cultural resources. Below is a summary of the evaluation performed.

4.1 Section 106 of the National Historic Preservation Act

A Cultural Resource Assessment Survey (CRAS), conducted in accordance with 36 CFR Part 800, was performed for the project, and the resources listed below were identified within the project Area of Potential Effect (APE). FDOT found that these resources do not meet the eligibility criteria for inclusion in the National Register of Historic Places (NRHP), and State Historic Preservation Officer (SHPO) concurred with this determination on 10/02/2020. Therefore, FDOT, in consultation with SHPO has determined that the proposed project will result in No Historic Properties Affected.

A CRAS was conducted in July 2020 and is included in the project file. The archaeological APE included the existing and proposed ROW, and the survey consisted of pedestrian survey and systematic shovel testing. The historic APE includes the archaeological APE and adjacent parcels along the mainline for up to 200 ft (61 meters [m]) from the centerline of the road, and a 100-ft (30.5-m) buffer around the pond site parcels.

The archaeological field survey resulted in the identification of one newly recorded prehistoric archaeological site, Cocoa Hill (8BR04221). FDOT determined that 8BR04221, as it is expressed within the APE, does not meet the minimum criteria for listing in the NRHP, however there is insufficient information to make a definitive determination about the site as a whole. No other sites or occurrences were identified.

The architectural survey resulted in the identification and evaluation of six historic resources within the APE, including one previously recorded resource and five newly recorded resources. The previously recorded historic resource is a structure, 2921 Slippery Rock Drive (8BR03331). The newly recorded historic resources include one resource group (8BR04195) and four structures (8BR04196-8BR04198 and 8BR04214). The historic structure at 2921 Slippery Rock Drive (8BR03331) was determined ineligible for the NRHP by the SHPO on November 23, 2015. Based on the current survey, FDOT determined that 8BR03331 remains ineligible for listing in the NRHP due to a lack of significant historic associations and architectural distinction. Furthermore, FDOT determined that the remaining five historic resources were ineligible for the NRHP due to a lack of significant historic associations and architectural distinction.

Given the results of the CRAS (July 2020), FDOT determined that the Preferred Alternative will have no effect to historic properties listed, eligible or potentially eligible for listing in the NRHP. The project falls within the area of interest for the Miccosukee Tribe of Indians of Florida, Muscogee (Creek) Nation, Poarch Band of Creek Indians, Seminole Nation of Oklahoma and Seminole Tribe of Florida (STOF) and the CRAS was forwarded for their review and comment on April 9, 2024. Transmittal letters are in the project file. Only the STOF provided a response on April 29, 2024. The STOF recommended avoidance of site 8BR04221 by all project activities as it could be not fully delineated. SHPO indicated in their 10/02/2020 concurrence with the CRAS that if alternative pond site Pond 2B is selected, SHPO requests additional close-interval testing in the east corner of the pond site to determine whether 8BR04221 extends into that area. The SHPO concurrence letter and STOF response letter are attached.

Pond 2F was selected as the Preferred Alternative, not Pond 2A nor 2B, therefore archaeological site 8BR04221 has been avoided and is no longer in the Preferred Alternative footprint.

4.2 Section 4(f) of the USDOT Act of 1966, as amended

The following evaluation was conducted pursuant to Section 4(f) of the U.S. Department of Transportation Act of 1966, as amended, and 23 CFR Part 774.

A Section 4(f) Document (November 2024) was prepared for two recreational resources located along the north side of SR 524 within the project limits and is attached. A summary is presented in **Table 4-1**.

Resource Name	Facility Type	Property Classification	Owner/Official with Jurisdiction	Recommended Outcome	OEM Action
Junny Rios Martinez Park	County Park	Park/Recreational Area	City of Cocoa/Brevard County Parks and Recreation Department	No Use	Determination 11-19-2024
Eastern Florida State College Fred Gay Golf Academy	Golf Training Facility	Park/Recreational Area	Eastern Florida State College	No Use	Determination 11-19-2024

Table 4-1: Section 4(f) Resources

4.3 Section 6(f) of the Land and Water Conservation Fund Act of 1965

There are no properties in the project area that are protected pursuant to Section 6(f) of the Land and Water Conservation Fund of 1965.

4.4 Recreational Areas and Protected Lands

There are no other protected public lands in the project area

5. Natural Resources

The project will not have significant impacts to natural resources. Below is a summary of the evaluation performed:

5.1 Protected Species and Habitat

The following evaluation was conducted pursuant to Section 7 of the Endangered Species Act of 1973 as amended as well as other applicable federal and state laws protecting wildlife and habitat.

A Natural Resources Evaluation (NRE) (March 2025) has been prepared and is included in the project file. A Preliminary Florida Scrub-Jay and Caracara Survey and Evaluation (November 2019) was prepared and submitted to USFWS on November 13, 2019 to document a habitat assessment for these two species as the project falls within the consultation area for both. This documentation is included in the NRE. A Preliminary Eastern Black Rail Habitat Suitability Evaluation (October 2024) was prepared and submitted to USFWS to address the Eastern black rail and is in the project file. All three of these documents contain species and habitat maps and an analysis of protected species potentially occurring within the project area.

Field surveys were performed in April 2019, February 2020, and September 2024 to evaluate proposed alternatives and address the occurrence, or potential occurrence, of wildlife and plant species listed as Threatened, Endangered, Species of Special Concern, or otherwise protected (protected species). Field surveys were conducted in accordance with the methodologies outlined by the United States Fish and Wildlife Service (USFWS) and Florida Fish and Wildlife Conservation Commission (FWC). The Florida Natural Areas Inventory (FNAI), USFWS, and FWC databases were consulted regarding current state and federally listed wildlife species, Species of Special Concern and other protected species that are known or have the potential to occur within certain habitats found in the region.

A summary of protected federal wildlife and their probability of occurrence within the project corridor and surrounding area is provided in **Table 5-1**. A summary of protected state wildlife and their probability of occurrence within the project corridor and surrounding area is provided in **Table 5-2**. Federally listed plant species identified using the USFWS's IPaC tool and State of Florida listed species through the Florida Department of Agriculture & Consumer Services (FDACS) identified by the FNAI Biodiversity Matrix as having the potential to occur along the project corridor are included in **Table 5-1** and **Table 5-2** respectively.

Federal Listed Species

The FDOT has made effect determinations and listed the probability of occurrence for federal endangered (E) and threatened (T) species in **Table 5-1**. One proposed endangered (PE) and one proposed threatened (PT) species are also included.

Scientific Name	Common Name	Listing Status	Probability of Occurrence	Effect Determination
Reptiles				
<i>Drymarchon corais couperi</i>	Eastern indigo snake	T	Moderate	MANLAA
Birds				
<i>Aphelocoma coerulescens</i>	Florida scrub jay	T	None	No effect
<i>Haliaeetus leucocephalus</i>	Bald eagle (1)	--	High	--
<i>Laterallus jamaicensis jamaicensis</i>	Eastern black rail	T	Low	MANLAA
<i>Mycteria americana</i>	Wood stork	DL	Low	--
<i>Polyborus plancus audubonii</i>	Audubon's crested caracara	T	Low	No effect

Plants				
Conradina brevifolia	Short-leaved rosemary	E	Low	No effect
Asimina pulchella	Beautiful pawpaw	E	Low	No effect
Polygala lewtonii	Lewton's polygala	E	Low	No effect
Warea carteri	Carter's mustard	E	Low	No effect
Mammals				
Perimyotis subflavus	Tricolored bat	PE	Low	--
Insects				
Danaus plexippus	Monarch butterfly	PT	Low	--
MANLAA = May Affect, Not Likely to Adversely Affect				
T = Threatened, E = Endangered				
PT=Proposed Threatened, PE = Proposed Endangered, -- = Not Listed, DL = Delisted				
(1) Protected under the Bald and Golden Eagles Protection Act (16 U.S.C. 668-668c)				

Table 5-1: Federal Listed Species Effect Determinations

The findings for federal threatened (T) and endangered (E) species are based on the project's NRE Report 2019 and 2024 Evaluations noted above, and no federal listed species observations during the April 2019, February 2020, and September 2024 field reviews.

There are ten federally listed faunal and plant species identified in the project species evaluation with a probability of occurrence within the project area. The threatened Audubon's crested caracara, and endangered short-leaved rosemary, beautiful pawpaw, Lewton's polygala and Carter's mustard have a low probability of occurrence due to lack of suitable habitat within the project area. The threatened Florida scrub jay has no probability of occurrence due to lack of suitable habitat. Therefore, a determination of *no effect* was made for these species.

The eastern indigo snake is listed by both the USFWS and FWC as Threatened. No eastern indigo snakes were observed during the field review of the corridor. No gopher tortoise burrows were identified within the project area and less than 25 acres of xeric habitat will be impacted by the construction of the project. Using the most recent version of the USFWS Eastern Indigo Snake Programmatic Effect Determination Key (key pathway: A>B>C>D>E> "MANLAA") the FDOT has determined the project *may affect, not likely to adversely affect* the eastern indigo snake. The FDOT commits to implementing the USFWS *Standard Protection Measures for the Eastern Indigo Snake* during site preparation and construction.

The USFWS has removed the Southeast U.S. distinct population segment of the wood stork from the Federal List of Endangered and Threatened Wildlife, effective March 12, 2026. Federal agencies (including FDOT under NEPA assignment) will no longer be required to consult with the Service under section 7 of the Act for the wood stork. The wood stork is now a state-listed threatened species, occurring on Florida's Endangered and Threatened Species List with state protections through the FWC who regulate and manage these species.

The eastern black rail is listed by both USFWS and FWC as Threatened. The project is located within the USFWS designated consultation area. The onsite survey in September 2024 was specifically focused on identifying and assessing potential eastern black rail foraging or nesting habitats, which include a variety of salt, brackish, and freshwater marsh habitats as well as wet prairie habitats. No eastern black rail were observed during the survey days within the project area. Of the twelve wetlands areas identified within the study area, only four of them contain potential eastern black rail habitat. No suitable habitat was observed within the footprint of the proposed impacts of the Preferred Alternative or within 25 feet of the proposed impacts. Eastern black rail habitat was limited to Wetland 1 located near the western extent of the project area within Pond 1A. The suitability of this habitat is low, due to the absence of appropriate vegetation structure, density and height. Based on the assessment, the proposed project is not anticipated to result in adverse effects on the eastern

black rail or their habitat. Therefore FDOT has determined the project *may affect, not likely to adversely affect* the eastern black rail.

The tricolored bat was proposed to be listed as endangered under the ESA by the USFWS on September 13, 2022. If the listing status of the tricolored bat is elevated by the USFWS to Threatened or Endangered and the Preferred Alternative contains suitable habitat, FDOT commits to no tree clearing or bridge/culvert work when day-time high temperatures are below 45 degrees, nor during maternity season (May 1st through July 15th). With implementation of the commitments the project *may affect, not likely to adversely affect* the tricolored bat. If tree clearing is required during these months, consultation will be reinitiated.

The monarch butterfly was proposed to be listed as threatened under the ESA by the USFWS on December 12, 2024. The occurrence of monarchs is expected to be limited, and incidental to the species moving through the area. The probability of occurrence is low. Further impact assessment and consultation with USFWS for this species will be required once a listing decision has been made.

The bald eagle is protected by the Bald and Golden Eagle Protection Act and the Migratory Bird Treaty Act. In addition, the bald eagle is protected in Florida through F.A.C. 68A-16.002. The Audubon Florida Eagle Watch bald eagle nest database identifies bald eagle nest (BE082) located approximately 600 feet south of the project corridor as documented in the NRE. The location of nest and the 660 foot buffer zone will be depicted on the project plans and modified special provisions will be provided to the contractor that will require them to coordinate with the USFWS to provide monitoring of the nest if work is required within the protective buffer zone during the nesting season (October 1 to May 15). Based on this assessment and provisions for coordination during permitting and construction, no adverse impacts are anticipated for the bald eagle.

USFWS Critical Habitat

The study area was assessed for Critical Habitat designated by Congress in 50 CFR Part 17. The project area does not include USFWS designated Critical Habitat for any species. Therefore, the project will have no adverse modifications of Critical Habitat.

State Listed Species

The FDOT has made effect determinations for state endangered (SE) and threatened (ST) species listed in **Table 5-2**.

Scientific Name	Common Name	Listing Status	Probability of Occurrence	Effect Determination
Reptiles				
Gopherus polyphemus	Gopher tortoise	ST	Low	No Adverse Effect Anticipated
Pituophis melanoleucus	Florida pine snake	ST	Low	No Adverse Effect Anticipated
Birds				
Antigone canadensis pratensis	Florida sandhill crane	ST	Low	No Adverse Effect Anticipated
Athene cunicularia floridana	Florida burrowing owl	ST	Low	No Effect Anticipated
Egretta caerulea	Little blue heron	ST	Moderate	No Effect Anticipated
Egretta tricolor	Tricolored heron	ST	Moderate	No Effect Anticipated
Falco sparverius paulus	Southeastern American kestrel	ST	Low	No Effect Anticipated
Mycteria americana	Wood stork	ST	Low	No Adverse Effect Anticipated
Platea Ajaja	Roseate spoonbill	ST	Moderate	No Effect Anticipated
Mammals				
Ursus americanus floridanus	Florida black bear (1)	--	Low	--
Plants				
Calopogon multiflorus	Many-flowered grass- pink	ST	Low	No Adverse Effect Anticipated

Carex tenax	Chapman's sedge	ST	Low	No Effect Anticipated
Centrosema arenicola	Sand butterfly pea	SE	Low	No Effect Anticipated
Cheiroglossa palmata	Handfern	SE	Low	No Effect Anticipated
Coelorachis tuberculosa	Piedmont joint grass	ST	Low	No Effect Anticipated
Coleataenia abscissa	Cutthroat grass	SE	Low	No Effect Anticipated
Conradina grandiflora	Large-flowered rosemary	ST	Low	No Effect Anticipated
Dicerandra thinicola	Titusville balm	SE	Low	No Effect Anticipated
Euphorbia cumulicola	Sand-dune spurge	SE	Low	No Effect Anticipated
Glandularia maritima	Coastal vervain	SE	Low	No Effect Anticipated
Glandularia tampensis	Tampa vervain	SE	Low	No Effect Anticipated
Lace-lip	Ladies'-tresses	ST	Low	No Effect Anticipated
Lechea cernua	Nodding pinweed	ST	Low	No Effect Anticipated
Lechea divaricata	Pine pinweed	SE	Low	No Effect Anticipated
Lilium caresbyi	Pine Lily	ST	Low	No Effect Anticipated
Linum carteri var. smallii	Small's flax	SE	Low	No Adverse Effect Anticipated
Nemastylis floridana	Celestial lily	SE	Low	No Adverse Effect Anticipated
Nolina atopocarpa	Florida beargrass	ST	Low	No Effect Anticipated
Orthochilus ecristata	Giant orchid	ST	Low	No Effect Anticipated
Pinguicula caerulea	Blueflower butterwort	ST	Low	No Effect Anticipated
Pinguicula lutea	Yellow flowered butterwort	ST	Low	No Effect Anticipated
Sarracenia minor	Hooded pitcherplant	ST	Low	No Effect Anticipated
Sporobolus vaseyi	Curtiss' sandgrass	ST	Low	No Effect Anticipated
ST = State Threatened, SE = State Endangered				
(1) Protected under the Florida Black Bear Conservation Rule (68A-4.009, F.A.C.)				

Table 5-2: State Listed Species Effect Determinations

Below is a summary of findings for state-designated endangered (SE) and threatened (ST) species consistent with the project's NRE. No state listed species observations were made during the April 2019, February 2020 and September 2024 field reviews.

The gopher tortoise is listed by the FWC as threatened. No gopher tortoises or their burrows have been observed during the field survey, and little suitable habitat for this species occurs within the project study area. However, a gopher tortoise survey will also be performed during the permitting phase of the project. Based on this assessment, the FDOT determined the potential for the gopher tortoise to occur within the project corridor is low with *no adverse effect anticipated* to the species due to the project.

The Florida pine snake is listed by the FWC as Threatened. No pine snakes have been observed during field review and little suitable habitat exists within the project area. Therefore, the FDOT determined the potential occurrence of the pine snake is low with *no adverse effect anticipated* to the species due to the project.

The Florida burrowing owl is listed as Threatened by the FWC. No burrowing owls were observed during the field surveys, and little suitable habitat for this species occurs within the project study area. Therefore, the FDOT determined the potential for occurrence for the burrowing owl is low with *no adverse effect anticipated* to the species due to the project.

Wading birds as a group are common to wetlands where they forage for small fish and invertebrates. Species that could be found in wetlands within the corridor include species such as little blue heron and tricolored heron which are listed as Threatened by the FWC. Review of the FWC Waterbird Colony Locator indicated that there are no known active wading bird colonies near the project corridor. However, minimal temporary impacts to wading bird foraging habitat are anticipated. If applicable, replacement foraging habitat will be provided onsite as part of the stormwater management system or through the purchase of herbaceous wetland mitigation. Therefore, FDOT determined this project to have *no adverse effect anticipated* on these species.

The Southeastern American kestrel requires three components for optimal habitat: large, open fields for foraging, snags for nesting, and snags, fence lines or telephone poles as perching sites from which to hunt. No kestrels were observed along the project corridor or within any pond sites and there is minimal habitat within the project area. Therefore, the FDOT determined the potential occurrence of the Southeastern kestrel is low with *no adverse effect anticipated* to the species due to the project.

The Florida sandhill crane typically nests in shallow freshwater marshes and forages on agricultural lands. They are listed as Threatened by FWC. No sandhill cranes were observed during field surveys and little suitable habitat exists within the project study area. Therefore, the FDOT determined the potential occurrence of the sandhill crane is low with *no adverse effect anticipated* to the species due to the project.

The roseate spoonbill is primarily a coastal species listed by the FWC as Threatened but can also be seen far inland. No roseate spoonbills, their nests or roosting sites were observed during the survey. However, minimal temporary impacts to roseate spoonbill foraging habitat are anticipated. If applicable, replacement foraging habitat will be provided onsite as part of the stormwater management system or through the purchase of herbaceous wetland mitigation. Therefore, the FDOT determined this project was determined to have *no adverse effect anticipated* on this species.

The wood stork is listed by FWC as Threatened. Prior to federal delisting, the project right of way was located within a USFWS designated wood stork Core Foraging Area (CFA). The nearest active wood stork nesting colony (SR 524/520) is located approximately 0.55 miles southwest of the project ROW. No wood storks were observed during survey days within the project area. However, minimal impacts to wood stork foraging habitat are anticipated. Prior to federal delisting, using the most recent version of the USFWS's Wood Stork Programmatic Concurrence Key (key pathway: A>B>C>D>E> "MANLAA") the FDOT determined the project may affect, not likely to adversely affect the wood stork. With federal delisting, the FDOT has changed the determination to *no adverse effect anticipated* on this species.

Many-flowered grass-pink is a wetland orchid that is listed as Threatened by the State of Florida. Some suitable habitat includes longleaf pine flatwoods with wiregrass and saw palmetto. Some pine flatwood habitat exists along the project corridor and within some of the pond site alternatives but no manyflowered grass pink was observed during the listed species surveys. Therefore, the FDOT determined this project to have *no adverse effect anticipated* on this species.

Piedmont Jointgrass is a perennial grass that is listed as Threatened by the State of Florida. Some suitable marsh habitat exists along the project corridor and one of the preferred pond alternatives, but no Piedmont Jointgrass was observed during the listed species surveys. Therefore, the FDOT determined the project to have *no adverse effect anticipated* on this species.

Tampa vervain is listed as Endangered by the State of Florida. Some suitable pine flatwood habitat exists along the project corridor and within some of the pond site alternatives but no Tampa vervain was observed during the listed species surveys. Therefore, the FDOT determined the project to have *no adverse effect anticipated* for this species.

Lace-lip Ladies'-tresses is a perennial herb in the orchid family that is listed as Threatened by the State of Florida. Suitable freshwater marsh habitat covers most of pond site 1A, but no Lace-lip ladies'-tresses were observed during the listed species surveys. Therefore, the FDOT determined the project to have *no adverse effect anticipated* on this species.

Pine Lily is a perennial wildflower that is listed as Threatened by the State of Florida. Some suitable wet prairie and pine flatwoods habitat exist along the project corridor and one of the preferred pond alternatives, but no Pine lily was observed

during the listed species surveys. Therefore, the FDOT determined the project to have *no adverse effect anticipated* on this species.

Small's flax is a wetland, annual herb that is listed as Endangered by the State of Florida. Some suitable pine flatwood habitat exists along the project corridor and within some of the pond site alternatives but no Small's flax was observed during the listed species surveys. Therefore, the FDOT determined the project to have *no adverse effect anticipated* for this species.

Celestial lily is a perennial herb typically found in wetlands that is listed as Endangered by the State of Florida. Some suitable marsh habitat exists along the project corridor and one of the pond site alternatives but no Celestial lily was observed during the listed species surveys. Therefore, the FDOT determined the project to have *no adverse effect anticipated* for this species.

Blueflower butterwort is an insectivorous wildflower that is listed as Threatened by the State of Florida. Some suitable wet prairie and pine flatwoods habitat exist along the project corridor and many of the pond sites, but no Blueflower butterwort was observed during the listed species surveys. Therefore, the FDOT determined the project to have *no adverse effect anticipated* on this species.

Yellow Flowered Butterwort is a perennial carnivorous herb that is listed as Threatened by the State of Florida. Some suitable wet prairie and pine flatwoods habitat exist along the project corridor and some of the pond sites, but no Yellow flowered butterwort was observed during the listed species surveys. Therefore, the FDOT determined the project to have *no adverse effect anticipated* on this species.

Hooded pitcherplant is a carnivorous clumping plant that is listed as Threatened by the State of Florida. Some suitable pine flatwoods habitat exists along the project corridor and freshwater marsh covers most of pond site 1A, but no Hooded pitcherplant was observed during the listed species surveys. Therefore, the FDOT determined the project to have *no adverse effect anticipated* on this species.

Other evaluated plants listed as Threatened by the State of Florida include: Curtiss' sandgrass, Chapman's sedge, Large-flowered rosemary, Nodding pinweed, Florida beargrass, Giant orchid; and as Endangered include: Sand butterfly pea, Sand-dune spurge, Short-leaved rosemary, Beautiful pawpaw, Titusville balm, Coastal vervain, Pine pinweed, Cutthroat grass, Lewton's Polygala, Carter's Mustard, and Handfern. During the field surveys, no listed plant species were observed within the project corridor. The probability of occurrence of these species is low. Therefore, the FDOT determined this project to have *no effect anticipated* on this species.

Agency Coordination

On December 11, 2019, USFWS concurred with the effect determinations outlined in the Preliminary Florida Scrub-Jay and Caracara Survey and Evaluation, that no other survey effort will be required, and consultation is complete. The December 11, 2019 USFWS concurrence letter is attached.

USFWS concurred with the Preliminary Eastern Black Rail Habitat Suitability Evaluation on December 2, 2024 effect determination and no additional surveys will be required for this species during the design phase of the project and consultation is complete. The December 2, 2024 USFWS concurrence letter is attached.

Based on the effect determinations and USFWS consultation keys, no additional consultation with USFWS is required.

On April 24, 2025, FWC agreed with the effect determinations in the updated NRE and supports the project implementation measures and commitments. The April 24, 2025 FWC letter is attached.

5.2 Wetlands and Other Surface Waters

The following evaluation was conducted pursuant to Presidential Executive Order 11990 of 1977 as amended, Protection of Wetlands and the USDOT Order 5660.1A, Preservation of the Nation's Wetlands.

The project was evaluated for impacts to wetlands and other surface waters and the findings are documented in the NRE (March 2025) which contains the associated habitat and wetland maps and is included in the project file.

The proposed roadway widening is situated within the existing ROW for SR 524, avoiding impacting wetlands adjacent to the ROW. The only ROW needed for the roadway improvements are for the proposed roundabouts, where no existing wetlands are located. As noted below in **Table 5-3**, there are wetland impacts associated with Pond Sites 1A and 2F, however alternative pond sites 2A and 2C were eliminated from consideration to avoid greater impacts. Further, Pond Sites 3A and 3B situated in the existing ROW were selected over the alternative Regional Pond A which would have resulted in wetland impacts. The Pond Siting Report (May 2023) provides locations of alternative pond sites that were not selected as preferred.

The jurisdictional extent of wetland and other surface water systems within the study corridor was approximated through the review of aerial photography, National Wetlands Inventory (NWI) data, U.S. Geological Survey Topographic Maps, Soils Maps, Land Use Maps, and field observations made during the April 2019 and February 2020 site visits. The wetland limits were identified in general accordance with federal and state regulations. In the event wetland boundaries differed between the two methods, the more landward extent was used to define that wetland system's boundary. Photographic documentation was used to capture the current condition of each wetland system and the Uniform Mitigation Assessment Method (UMAM, Chapter 62-345 F.A.C.) was used to quantify each system's condition.

Thirteen wetlands are located within the study area, which consist of Mixed Wetland Hardwoods, Wetland Forested Mixed, Freshwater Marsh, Wet Prairie, and Emergent Aquatic Vegetation. Wetland and other surface water impacts is estimated at approximately 7.36 acres with the proposed improvements of the Preferred Alternative and preferred stormwater pond sites.

Table 5-3 provides a summary of the wetland impacts for the Preferred Alternative. **Table 5-4** provides a summary of the wetland impacts including their FLUCCS code, NWI classification, size, and UMAM functional loss for the Preferred Alternative.

Roadway Elements & Pond Sites	Forested Wetland Impacts (acres)	Herbaceous Wetland Impacts (acres)	Total Wetland Impacts (acres)
Roadway Widening	0.17	1.43	1.6
I-95 Diverging Diamond Interchange	0	0	0
Cox Road Roundabout	0	0	0
London Boulevard Roundabout	0	0	0
Pond Site 1A	0	3.87	3.87
Pond Site 2F	1.89	0	1.89
Pond Site 3A	0	0	0
Pond Site 3B	0	0	0
Total Impacts	2.06	5.30	7.36

Table 5-3: Potential Wetland Impacts of the Preferred Alternative

Roadway Elements & Pond Sites	Wetland Number	FLUCCS	NWI Code	Impact (acres)	Functional Loss
Preferred Alternative - SR 524	W-3	643	PEM1C	0.27	0.16
	W-10	617	PFO1/4C	0.17	0.12
	W-11	643	PEM1F	1.16	0.66
I-95 Diverging Diamond Interchange	W-1	641	PEM1C	0	0
	W-12	630	PFO1C	0	0
Cox Road Roundabout	N/A	N/A	N/A	0	0
London Boulevard Roundabout	N/A	N/A	N/A	0	0
Pond Site 1A	W-1	641	PEM1C	3.87	2.59
Pond Site 2F	W-10	617	PFO1/4C	1.89	1.38
Pond Site 3A	N/A	N/A	N/A	0	0
Pond Site 3B	N/A	N/A	N/A	0	0
Totals				7.36	4.91

Table 5-4: Preferred Alternative Wetland Impacts & UMAM Summary

Secondary wetland impacts for the roadway improvements are limited, since most of the area within the existing ROW had previously been cleared. Secondary wetland impacts that may result from the construction of the proposed pond sites will be addressed in the design phase by creating onsite upland buffers averaging 25 feet in width along the wetland boundary. In areas where buffers are not feasible, secondary impacts will be calculated and mitigation will be provided in accordance with Section 373.4137 F.S. Cumulative impacts are not anticipated because mitigation will be within the same drainage/mitigation basin as the proposed wetland impacts.

Except for pond improvements, the Preferred Alternative of an urban roadway typical section combined with a 200-ft. ROW corridor will have minimal impacts on existing wetlands. Specific wetlands impacts are approximately 1.6 acres for the roadway improvements in the Preferred Alternative, while wetland and surface water impacts are approximately 5.76 acres for pond sites. Potential impacts to wetlands were assessed using UMAM to determine mitigation needs. Avoidance and minimization measures have been applied with the development of the Preferred Alternative, and mitigation will be provided for any unavoidable wetland impacts.

Wetland impacts which will result from the construction of this project will be mitigated pursuant to Section 373.4137, F.S., to satisfy all mitigation requirements of Part IV of Chapter 373, F.S., and 33 U.S.C. Section 1344. Under the provisions of Section 373.4137 F.S., the St. Johns River Water Management District (SJRWMD) is funded to provide for specific FDOT project impacts through a corresponding mitigation project within the SJRWMD's overall approved regional mitigation plan.

If the SJRWMD is unable to provide appropriate mitigation, mitigation credits from a permitted wetland mitigation bank are available. The southern half of the SR 524 project falls within Mitigation Basin 18, St. Johns River (Canaveral Marshes to Wekiva) and the northern half of the project is in Mitigation Basin 20, Southern St. Johns River. Mitigation banks with available credits located within Mitigation Basin 18 include Lake Monroe, Tosohatchee, Farmton, Colbert-Cameron, and TM Econ. Mitigation banks with available credits located within Mitigation Basin 20 include Mary A. Ranch, Farmton, and Lake Washington.

Any surface water impacts which will result from the construction of this project will be mitigated through onsite in-kind replacement. The proposed stormwater treatment and conveyance system will maintain existing surface water function. Temporary functional loss will occur during construction, but no permanent direct, secondary, or cumulative impacts are

anticipated.

A final mitigation plan for the project will be developed during the design and permitting phase with input from FDOT, SJRWMD, and US Army Corps of Engineers (USACE).

Wetlands Finding

This Wetlands Finding was made in accordance with Executive Order 11990. Wetland impacts are expected to be minor and will be finalized during the permitting process. It is determined that there is no practicable alternative to the proposed construction in wetlands and that the proposed action will include all practicable measures to minimize harm to wetlands. The proposed project will have no significant short-term or long-term adverse impacts to wetlands. Therefore, the proposed project is expected to have no significant impacts to wetlands and other surface waters.

5.3 Essential Fish Habitat (EFH)

There is no Essential Fish Habitat (EFH) in the project area.

5.4 Floodplains

Floodplain impacts resulting from the project were evaluated pursuant to Executive Order 11988 of 1977, Floodplain Management.

A Location Hydraulics Report (LHR-May 2023) was prepared for this project and is included in the project file. The LHR provides a detailed discussion of the potential floodplain encroachments and preliminary cross-drain evaluation. It was concluded the project will impact approximately 0.11 acre-feet of the 100-year floodplain and 2.49 acre-feet of the 10-year floodplain based on the Preferred Alternative. These impacts are minimal compared to the overall extent of the floodplain; therefore, it was determined the floodplain encroachment is "minimal." Minimal encroachments on a floodplain occur when there is a floodplain involvement, but the impacts on human life, transportation facilities, and natural and beneficial floodplain values are not significant and can be resolved with minimal efforts.

Floodplain Finding: The proposed cross drains and floodplain compensation areas will perform hydraulically in a manner equal to or greater than the existing condition, and backwater surface elevations are not expected to increase. As a result, there will be no significant change in flood risk, and there will not be a significant change in the potential for interruption or termination of emergency service or in emergency evacuation routes. It has been determined, through consultation with local, state, and federal water resources and floodplain management agencies that there is no regulatory floodway involvement on the proposed project and that the project will not support base floodplain development that is incompatible with existing floodplain management programs. Therefore, it has been determined that this encroachment is not significant.

5.5 Sole Source Aquifer

There is no Sole Source Aquifer associated with this project.

5.6 Water Resources

A Pond Siting Report (May 2023), LHR (May 2023) and Water Quality Impact Evaluation (WQIE-December 2024) were prepared as part of the PD&E Study and are included in the project file.

Water Body Identification (WBID) 28935 is listed as impaired for silver and fecal coliform. WBID 3056 is not listed as impaired. There are currently no adopted Total Maximum Daily Load levels for WBID 28935, but one is under development for silver by the Florida Department of Environmental Protection (FDEP). Since Mud Lake Outlet flows to the St. Johns River above Puzzle Lake (South Segment) the proposed stormwater ponds will require nutrient loading calculations to show a net improvement for nutrients. The stormwater ponds in the Preferred Alternative will be designed to meet state water quality and quantity requirements. Proposed stormwater management systems will be designed to meet these requirements in the design phase. In addition, all relevant agency coordination will occur, and permits will be obtained for the design of the stormwater systems in accordance with the SJRWMD criteria for attenuation/flood control and treatment.

5.7 Aquatic Preserves

There are no aquatic preserves in the project area.

5.8 Outstanding Florida Waters

There are no Outstanding Florida Waters (OFW) in the project area.

5.9 Wild and Scenic Rivers

There are no designated Wild and Scenic Rivers or other protected rivers in the project area.

5.10 Coastal Barrier Resources

It has been determined that this project is neither in the vicinity of, nor leads directly to a designated coastal barrier resource unit pursuant to the Coastal Barrier Resources Act of 1982 (CBRA) and the Coastal Barrier Improvement Act of 1990 (CBIA).

6. Physical Resources

The project will not have significant impacts to physical resources. Below is a summary of the evaluation performed for these resources.

6.1 Highway Traffic Noise

The following evaluation was conducted pursuant to 23 CFR 772 Procedures for Abatement of Highway Traffic Noise and Construction Noise, and Section 335.17, F.S., State highway construction; means of noise abatement.

The project is identified as a Type I project pursuant to 23 CFR Part 772 and 335.17, F.S. A Noise Study Report (NSR-September 2024, revised February 2026) was prepared for this project and is included in the project file. The NSR analyzed 149 noise receptors representing 246 residential and seven non-residential special land uses. The analysis predicted that under the Preferred Alternative, 63 sites would meet or exceed the 66.0 dB(A) Noise Abatement Criterion. Overall, noise levels increase an average of 0.7 decibel A-weighted [dB(A)]; none of the increases are considered substantial (i.e., 15 or more dB(A) over existing conditions).

Noise abatement consideration was given to all 63 sites impacted by the Preferred Alternative. Traffic management measures, modifications to the roadway alignment, and buffer zones were considered as abatement measures, but determined to not be reasonable or feasible methods to provide noise abatement. Three noise barrier systems were evaluated to provide abatement for the impacts. Optimizing the barrier height includes consideration of insertion loss, cost, and community context/aesthetics. A barrier height of 22-ft. is recommended for barrier systems at Integra Trails (EB1) and a 14-foot height at Cocoa Pines (WB1) and Cocoa North Villas (WB2). The barrier analysis and preliminary barrier locations are included in the NSR. The proposed barriers provide abatement for 51 of the 63 impacted residences as well as 34 non-impacted residences. A map showing the impacted receptors and proposed barriers is attached and included in the NSR.

The FDOT is committed to the construction of feasible and reasonable noise abatement measures at the noise impacted locations identified as EB1, WB1 and WB2 contingent upon the following conditions:

1. Final recommendations on the construction of abatement measures are determined during the project's final design and through the public involvement process;
2. Detailed noise analyses during the final design process support the need, feasibility and reasonableness of providing abatement;
3. Cost analysis indicates that the cost of the noise barrier(s) will not exceed the cost reasonable criterion;
4. Community input supporting types, heights, and locations of the noise barrier(s) is provided to the District Office; and
5. Safety and engineering aspects as related to the roadway user and the adjacent property owner have been reviewed and any conflicts or issues resolved.

6.2 Air Quality

This project is not expected to create adverse impacts on air quality because the project area is in attainment for all National Ambient Air Quality Standards (NAAQS) and because the project is expected to improve the Level of Service (LOS) and reduce delay and congestion on all facilities within the study area.

Construction activities may cause short-term air quality impacts in the form of dust from earthwork and unpaved roads.

These impacts will be minimized by adherence to applicable state regulations and to applicable FDOT Standard Specifications for Road and Bridge Construction.

6.3 Contamination

A Contamination Screening Evaluation Report (CSER, October 2024), included in the project file, was prepared to assess the risk of encountering petroleum or hazardous substance contamination of soil, groundwater, surface water, or sediment that could adversely affect this project. Based on the review of the Environmental Database Reports (EDR) Radius Map Report, site reconnaissance, aerial photograph review, city directory review, interviews, and file review conducted on the FDEP's online database, a total of 15 sites with potential for hazardous material or petroleum impact to the soil and/or groundwater were identified and documented in the CSER. Of the 15 potential sites identified, three were rated No Risk, five Low Risk, four Medium Risk, and three High Risk. **Table 6-1** provides a listing of No, Low, Medium and High risk sites.

Site Number	Name	Address	Risk Rating
3	Mobil Oil Corp.	5555 SR 524, Cocoa, FL	High
4	Sunrise Food Mart	5550 SR 524, Cocoa, FL	High
7	Sunshine Food Mart	4900 SR 524, Cocoa, FL	High
6	Flying J	1101 Friday Road, Cocoa, FL	Medium
9	Sunrise Food Mart	4301 SR 524, Cocoa, FL	Medium
10	7-Eleven	3500 SR 524, Cocoa, FL	Medium
15	7-Eleven	2201 SR 524, Cocoa, FL	Medium
1	Days Inn	5600 SR 524, Cocoa, FL	Low
11	CVS Pharmacy	2324 SR 524, Cocoa, FL	Low
12	Pinch A Penny	2311 SR 524, Cocoa, FL	Low
13	Publix Super Market	2301 SR 524, Cocoa, FL	Low
14	The Home Depot	2300 SR 524, Cocoa, FL	Low
2	Brevard Ohio Corp.	5580 SR 524, Cocoa, FL	No
5	Atlantic Truck Lines	I-95 and SR 524, Cocoa, FL	No
8	Chevron	4880 SR 524, Cocoa, FL	No

Table 6-1: Potential Contamination Sites by Site Rating

The roadway improvements were developed to maximize the use of existing ROW which minimizes the involvement with potential contamination sites. Pond site 1A was selected in basin 1 to avoid impacts to site 7. A portion of Pond site 2F is located on the same parcel as potential contamination site number 9, a gas station, which is rated as Medium risk. This parcel is identified as a business relocation. Pond sites 3A and 3B were selected due to being situated within FDOT ROW. **Table 6-2** shows the risk rating for the preferred pond sites that are shown in **Figure 1-5**.

Pond Site	Risk Rating (Contamination Site Number)
1A	No
2F	Medium (9)
3A	No
3B	No

Table 6-2: Potential Contamination Risk Rating of Preferred Pond Sites

For those locations with a risk rating of "Medium" or "High" that are likely to be impacted by construction, including preferred pond sites, a Level II field screening will be conducted during the design phase.

6.4 Utilities and Railroads

A Utility Assessment Report (UAR) was prepared in February 2023 and is included in the project file to document the presence of existing utilities within the SR 524 corridor. Overhead facilities are located at or near the ROW line, which reduces the potential impact for these facilities. Typical impacts are anticipated for underground facilities due to proposed improvements associated with drainage, signalization, lighting, etc.

Sunshine One Call combined with preliminary utility coordination efforts have identified 11 potential Utility Owner Agencies (UAOs) along the corridor as shown in **Table 6-3**. Preliminary utility coordination and investigation effort was conducted through written and verbal communications with the existing utility owners. A Sunshine 811 Florida Design Ticket System listing of existing utility owners was acquired on October 12, 2021. Coordination with each UAO will continue in future phases of project development.

Utility Agency	Utility Type
AT&T	Communication
Charter Communication	Communication
City of Cocoa (Water)	Water & Reclaimed Water
City of Cocoa (Sewer)	Sewer
Crown Castle	Communication
Florida Gas Transmission (FGT)	Gas Transmission
Florida City Gas	Gas
Florida Power and Light	Electric
Verizon (f/k/a MCI)	Communication
Florida Turnpike Traffic (FTE)	Communication
Uniti Fiber, LLC	Communication

Table 6-3: Existing Utilities

There are no railroads on this project.

6.5 Construction

Construction activities may cause short-term air quality impacts in the form of dust from earthwork and unpaved roads. These impacts will be minimized by adherence to applicable state regulations and to applicable FDOT *Standard Specifications for Road and Bridge Construction*. A National Pollution Discharge Elimination System (NPDES) permit will be acquired along with development of the required Stormwater Runoff Control Concept during the design phase.

Construction activities for the Preferred Alternative will have temporary noise, vibration, water quality, traffic flow, safety, access, and visual effects for the travelers within the immediate vicinity of the project. These effects will be minimized through application of the FDOT *Standard Specifications for Road and Bridge Construction*. Water quality impacts from construction will be minimized through the use of Best Management Practices including, but not limited to, construction phasing, sediment barriers, silt fences, and other techniques identified during design and permitting by the regulatory agencies and later during construction by the selected contractor.

Maintenance of traffic and sequence of construction will be planned and scheduled so as to minimize traffic delays throughout the project. Signage will be used as appropriate to provide pertinent information to the traveling public. The local news media will be notified in advance of road closings and other construction related activities to allow for the planning of alternate routes. Access to local properties, businesses and residences will be maintained to the extent

practical through controlled construction scheduling and the implementation of the project's specific Traffic Control Plan. Aesthetic impacts will be temporary and could consist of the staging of construction equipment and materials.

A Conceptual Transportation Management Plan that will include traffic control and potential work zone management strategies will be developed during the design phase which will include a temporary traffic control plan, transportation operations plan and public information plan. The Temporary Traffic Control Plan will be developed during final design of this project following current FDOT Design Manual and Standard Plans criteria and will address:

- Lane closure analysis identifying restrictions for SR 524 and I-95 travel lane closures during work hours, holidays, and special events
- Temporary overnight detour traffic routing may be needed for SR 524 traffic under the I-95 or along I-95 during removal of overhead elements of the existing bridge and setting of bridge beams for the new bridges. A detour routing plan will be developed during the final design of this project.
- Detailed traffic control schemes addressing construction of I-95 SR 524 DDI, Cox Road and London Boulevard roundabouts, bridge demolition and new bridge construction

The Preferred Alternative is anticipated to have no substantial impact to residents, business owners and road users during construction.

7. Engineering Analysis Support

The engineering analysis supporting this environmental document is contained within the Preliminary Engineering Report (May 2026) .

8. Permits

The following environmental permits are anticipated for this project:

Federal Permit(s)

USACE Section 10 or Section 404 Permit

Status

To be acquired

State Permit(s)

DEP or WMD Environmental Resource Permit (ERP)

DEP National Pollutant Discharge Elimination System Permit

Status

To be acquired

9. Public Involvement

The following is a summary of public involvement activities conducted for this project:

Summary of Activities Other than the Public Hearing

Public involvement activities have been integrated into the PD&E Study process, allowing property owners, residents, businesses, government entities, and agencies to share their ideas and concerns with the study team. The Efficient Transportation Decision Making (ETDM) process was used to obtain environmental agencies comments on the project's potential impacts. Additional coordination was conducted through meetings with stakeholders, SCTPO, Brevard County, and the City of Cocoa. Multiple meetings were held with each stakeholder to identify their specific concerns regarding potential alternative improvements evaluated in this PD&E Study. Input gained from the ETDM process, public meetings, and stakeholder meetings was used to inform the decision-making process, develop alternative concepts, and reach final recommendations.

A Public Involvement Plan (PIP-December 2018) was developed at the beginning of the PD&E Study that defined the approach for engaging and informing stakeholder and the community about this project and is included in the project file. Affected stakeholders, elected and public officials, state and federal environmental resource agencies, and local businesses and communities were identified. Newsletters were distributed and an alternatives public meeting was held during the study process to seek and gather valuable information from individuals who use the roadway corridor the most. All project activities complied with Title VI of the 1964 Civil Rights Act and related statutes, as referenced in FDOT's Non-Discrimination Policy, Topic Number 001-275-006, and implemented procedure Topic Number 275-010-010. A Comments and Coordination Report (CCR-April 2026), included in the project files, documents all agency coordination, public involvement activities and public comments.

Advance Notification

An Advance Notification Package was forwarded to the Florida State Clearinghouse at the FDEP, as well as local and Federal agencies on December 8, 2017, in accordance with Executive Order 95-359. The package specified that the project had been screened through the ETDM process. FDEP provided no comments and the USACE responded with no issues and concurred with the initial assessment of Wetlands and Surface Water and Navigation issues.

Public Website

At the start of the PD&E Study, project details, including contact information and study documents, were made available at: https://www.cflroads.com/project/437983-1/SR_524_Corridor_Planning_Study.

Project Kickoff Notification Letter

A kickoff newsletter was mailed to 698 citizens (elected and appointed officials, residents and property owners, and interested parties identified during the planning phase) on March 29, 2019.

Alternatives Public Meeting

The alternatives public meeting was held on Tuesday, May 4, 2021, from 5:30 p.m. to 7:30 p.m. at the Cocoa Civic Center, 430 Delannoy Avenue, Cocoa, FL. This was a hybrid public meeting, offering two options for the community to participate. Interested persons could either join the Virtual Public Meeting (VPM) from a computer, tablet, or cell phone or participate in person by going to the Cocoa Civic Center. All participants were provided the same display materials and presentation.

The meeting was advertised through several methods, including:

- Advertisement in the Florida Administrative Register, Vol 47/79 on April 23, 2021
- Direct mail notification to approximately 720 property owners/tenants
- Notification letters and emails to approximately 120 state and local elected and appointed public officials and other agencies (including Environmental Technical Advisory Team [ETAT] members and Tribal contacts)
- Display advertisement in the Friday, April 23, 2021, edition of the Florida Today
- Press releases to local media outlets
- Announcement on the FDOT website
- Announcement on the project website: <https://www.cflroads.com/project/437983-1>
- Coordination with local homeowner's associations and communities.

The meeting was conducted in an open house format, with a looping presentation provided for in-person citizen viewing at any time. The public was invited to attend in person between 5:30 pm and 7:30 pm. The VPM opened at 5:30 pm, and the presentation began at 5:45 pm. A handout with project information and details was prepared and distributed to all attendees.

Twenty citizens (including City and County representatives) and 16 project team members signed in at the in person public meeting. While 51 people registered to attend the VPM, 37 people were in attendance during all or part of the virtual option.

Thirty-six comments were received through May 18, 2021:

- 12 comments were received during the public meeting - 4 written comment forms from in-person attendees and 8 written comments/questions from virtual attendees
- 24 emailed comments were received after the meeting

Comments provided both support and opposition to specific typical section alternatives and intersection improvement options presented. No opposition to the SR 524 widening was expressed during the public meeting or comment period. All comments received were taken into account in the development of the project alternatives.

Local Agency Coordination Meetings

Individual meetings were conducted with the stakeholders involved along the corridor, including SCTPO, Brevard County, and the City of Cocoa. Four conceptual design meetings were held with these local agencies to analyze corridor typical sections, alignments, and intersection improvements. Meetings with the SCTPO, Brevard County, and City of Cocoa were conducted on January 31, 2019 (Project Kick-Off) and February 19, 2020 (Update Meeting #1). Additional in person presentations were provided to the SCTPO Citizens Advisory Committee/Technical Advisory Committee (CAC/TAC) and the SCTPO Board on September 8, 2021, and September 9, 2021.

Date of Public Hearing: 01/20/2026

Summary of Public Hearing

A public hearing was conducted both virtually on January 20, 2026 and in-person on January 22, 2026 to present information to and receive public input from interested persons regarding the preferred alternative and evaluation performed to date. The virtual session was held using GotoWebinar starting at 5:30 p.m. where attendees were provided an introductory video and instructions on how to access displays and other materials prior to the formal presentation and public comment period beginning at 6:00 p.m. Advance registration was required. The in-person public hearing session was held at the Space Coast Convention Center at the Holiday Inn Express & Suites Cocoa, 301 Tucker Lane, Cocoa, FL 32926 starting at 5:30 p.m. with an open house for the public to view displays and a formal presentation and public

comment period beginning at 6:00 p.m.

The public hearing notices consisted of a public hearing handout and public hearing location map that were mailed to property owners and distributed to elected and agency officials and other stakeholders in December 2025. Notices were posted on the FDOT Public Involvement webpage on December 19, 2025 and on the project website on CFLRoads.com on December 29, 2025. Notices were placed for the public hearing in the Florida Administrative Register on January 13, 2026, and in the online and hard copy editions of the Florida Today local newspaper on December 28, 2025 and January 8, 2026.

A total of 65 citizens (non-staff) pre-registered for the virtual session and 35 of them attended the Public Hearing virtually on January 20, 2026. Twelve citizens submitted written questions and one person provided a verbal comment during the virtual session of the Public Hearing. A total of 118 citizens (non-staff) signed in and attended the in-person Public Hearing session on January 22, 2026. Comment forms were received from 51 individuals and 12 individuals made verbal comments during the formal public comment session. Twenty-two individuals submitted comments by email during the public comment period, which ended on February 2, 2026. Overall, there were a total of 93 comments received from 79 unique members of the public. Public hearing displays, the legal display advertisements, notices, sign-in sheets, display graphics, PowerPoint slides for both the virtual and in-person sessions, and attendance rosters are included in Appendix G of the CCR.

The official *Public Hearing Certifications* (February 2026) and the *Public Hearing Transcripts* (February 2026) are attached and included in the project file. Public comments received during the public hearing comment period and responses provided are detailed in the Appendix I of the CCR.

The public hearing comments covered a wide range of topics. The comments provided general support for the project to relieve traffic congestion through the addition of travel lanes and to provide pedestrian and bicycle accommodations. A complete table showing all comments received including the method received is included in Appendix I of the CCR. In general, multiple individuals provided comments covering these general topics:

- Proposed roundabouts at Cox Road and London Boulevard
- Proposed traffic signal at Friday Road (south) at the entrance to Lost Lakes community and gas station at intersection
- Truck and trailer accommodations
- Proposed typical section elements including median, shoulder or lane widths
- Proposed shared use path, pedestrian, bicycle, golf cart, e-bike accommodations
- Proposed intersection improvements at Westminster Boulevard and median opening at Coventry Court
- Project schedule and funding

FDOT answered all questions raised by the public during the public comment period. Responses were provided to all individuals making public comments and are documented in the CCR. The concerns raised from the public were considered. Responses included providing information on the analysis performed and conceptual design features of the Preferred Alternative. Conceptual design features of the proposed roundabouts were noted to address concerns about safety and accommodation of large vehicles and trailers. These conceptual design features at both roundabout locations include a traversable inner truck apron for front or rear wheel over-tracking of large vehicles, and wider roundabout lanes than the approaching roadway lanes to provide extra space between adjacent vehicles. It was noted that the public concerns will continue to be considered as the design phase progresses.

10. Commitments Summary

1. One recorded prehistoric archaeological site, Cocoa Hill (8BR04221) was identified in proximity to alternative Pond site 2B and an access road leading to Pond site 2A. The Preferred Alternative includes Pond site 2F as a preferred pond site in basin 2. If there are changes during the design phase that render Pond sites 2A or 2B under further consideration, coordination with SHPO and additional close-interval testing will take place to determine the extent of Cocoa Hill (8BR04221). In addition, as the site could be not fully delineated, the Seminole Tribe of Florida recommended avoidance of 8BR04221 by all proposed project activities.
2. The most recent USFWS *Standard Protection Measures for the Eastern Indigo Snake* will be adhered to during the construction.
3. Coordination with USFWS will be initiated if construction within the 660-foot protection buffer is proposed for nest BE082 during the bald eagle nesting season (October 1 to May 15). If required FDOT or their representative will monitor the nest according to the most recent USFWS *Bald Eagle Monitoring Guidelines*.
4. The anticipated effect determination for the tricolored bat is "may affect not likely to adversely affect". As the timeline for construction is better defined, FDOT will adhere to the applicable commitment below:
 - Upon listing of the tricolored bat, if the project contains suitable habitat and requires tree trimming and/or clearing, FDOT will not conduct tree trimming/clearing activities during the tricolored bat pup season (May 1st to July 15th) and when bats may be in torpor (when temperatures are below 45 degrees Fahrenheit).
 - Upon listing of the tricolored bat, if the project contains suitable habitat and FDOT needs to trim or clear trees or perform work on bridges/culverts during the maternity season and/or when the temperature is below 45 degrees Fahrenheit, then FDOT will survey the project area for evidence of the tricolored bat. The *Indiana Bat and Northern Long-eared Bat Survey Guidance* (USFWS), Appendix J acoustic survey protocol in the year-round range (mist netting is not being conducted in Florida at this time), will be used for areas with tree trimming/clearing. For bridges and culverts, the *Indiana Bat and Northern Long-eared Bat Survey Guidance*, Appendix K, *Assessing Bridges and Culverts for Bats*, will be used.
1. If the surveys result in no tricolored bats detected, then FDOT can proceed with the project activities. Negative results from bridge/culvert surveys are valid for 2 years. Negative results for acoustic surveys are valid for 5 years. However, negative results for either survey may be invalidated if additional tricolored bat survey data is submitted to USFWS showing presence of the species within the vicinity of the project area. Additional survey work by FDOT, or application of the avoidance and minimization measures noted in the first bullet point above, may be required if updated detections are reported, and may result in reinitiation of consultation with USFWS.
2. If the surveys result in positive detections of the tricolored bat, FDOT will implement conservation measures such as: not conducting tree trimming/clearing activities during the tricolored bat pup season (May 1st to July 15th) when pups are not volant and not able to escape disturbance; similarly avoid tree trimming/clearing activities when the temperatures are below 45 degrees Fahrenheit when bats may be in torpor and unresponsive to disturbance.

5. The FDOT is committed to the construction of feasible and reasonable noise abatement measures at the noise impacted locations identified as EB1, WB1 and WB2 contingent upon the following conditions:
Final recommendations on the construction of abatement measures are determined during the project's final design and through the public involvement process; Detailed noise analyses during the final design process support the need, feasibility and reasonableness of providing abatement; Cost analysis indicates that the cost of the noise barrier(s) will not exceed the cost reasonable criterion; Community input supporting types, heights, and locations of the noise barrier(s) is provided to the District Office; and Safety and engineering aspects as related to the roadway user and the adjacent property owner have been reviewed and any conflicts or issues resolved.

11. Technical Materials

The following technical materials have been prepared to support this Environmental Document and are included in the Project File.

Conceptual Stage Relocation Plan (October 2024)
Sociocultural Data Report (February 2025)
Cultural Resources Assessment Survey (July 2020)
Natural Resources Evaluation (March 2025)
NRE Eastern Black Rail Evaluation Memo (October 2024)
Location Hydraulic Report (May 2023)
Pond Siting Report (May 2023)
Natural Resources Evaluation Update Memo (April 2024)
Water Quality Impact Evaluation (December 2024)
Contamination Screening Evaluation Report (October 2024)
Utility Assessment Report (February 2023)
Noise Study Report (Revised February 2026)
Value Engineering Study Report (November 2021)
Value Engineering Study Resolution Memo (November 2021)
Project Traffic Analysis Report (July 2019)
Project Traffic Analysis Report Reevaluation Memo (April 2024)
Interchange Modification Report (February 2022)
Intersection Control Evaluation (December 2024)
Preliminary Engineering Report (May 2026)
Public Involvement Plan (December 2018)
Comments and Coordination Report (April 2026)

Attachments

Planning Consistency

Project Plan Consistency Documentation (February 2026)

Community Effects

Figure 3-1: Existing Land Use Map

Figure 3-2: Future Land Use Map - City of Cocoa

Figure 3-3: Future Land Use Map - Brevard County

Farmland Conversion Impact Rating Form (December 2024)

Cultural Resources

SHPO Concurrence Letter (October 2020)

Seminole Tribe of FL Response (April 2024)

Section 4(f) Report

Natural Resources

USFWS Concurrence on Scrub-jay and Caracara Survey (December 2019)

USFWS Concurrence on Eastern Black Rail Evaluation (December 2024)

FWC Letter on NRE Effect Determinations (April 2025)

Physical Resources

Noise Barriers Map

Public Involvement

Public Hearing Certification-Virtual Session Jan 20-2026

Public Hearing Certification-In-person Session Jan 22-2026

Public Hearing Transcript - Virtual Session (February 2026)

Public Hearing Transcript - In-Person Session (February 2026)

Planning Consistency Appendix

Contents:

Project Plan Consistency Documentation (February 2026)



Space Coast Transportation Planning Organization

RESOLUTION # 26-07

Advance 2050 Long Range Transportation Plan

A RESOLUTION, adopting the Space Coast Transportation Planning Organization Advance 2050 Long Range Transportation Plan (LRTP) for the Palm Bay-Melbourne and Titusville Urbanized Areas.

WHEREAS, the Space Coast Transportation Planning Organization is the designated and constituted body responsible for the urban transportation planning and programming process for the Palm Bay-Melbourne and Titusville Urbanized Areas; and

WHEREAS, Federal regulations 23 C.F.R. 450.322, 450.324, 450.306 and FL State Statute 339.175 outline the requirements for MPOs to develop LRTPs through a performance-driven, outcome-based approach to planning for metropolitan areas of the State. The metropolitan transportation planning process shall be continuous, cooperative, and comprehensive; it should also provide for the consideration and implementation of projects, strategies, and services that will address all required factors; and

WHEREAS, the LRTP was developed with local, regional, state, multi-modal and environmental agencies that depicts an accurate representation of the areas priorities as developed through the planning process carried on cooperatively in accordance with the provisions of 23 U.S.C. 134; and

WHEREAS, the LRTP solicited extensive public input and involvement throughout the Plan's development including a forty-five (45) day review of the draft Plan and cost feasible projects; and

WHEREAS, The LRTP reflects fiscal constraint through a cost feasible plan that addresses the Space Coast area's transportation needs and priorities; and

NOW THEREFORE, BE IT RESOLVED by the Space Coast Transportation Planning Organization adopts the Advance 2050 Long Range Transportation Plan and authorizes staff to submit the LRTP to all appropriate individuals and agencies.


Passed and duly adopted at a regular meeting of the Space Coast Transportation Planning Organization Governing Board on the 11th day of September, 2025.

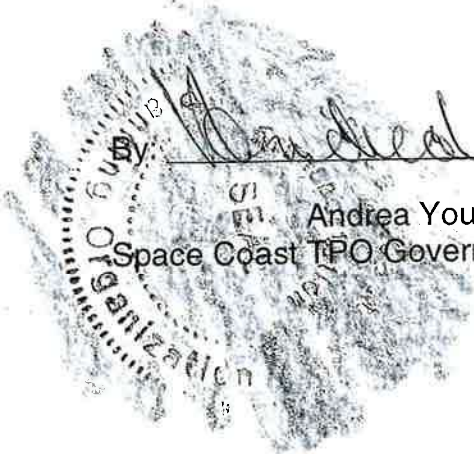



Space Coast Transportation Planning Organization

Certificate

The undersigned duly qualified as Chair of the Space Coast Transportation Planning Organization Governing Board certifies that the foregoing is a true and correct copy of a Resolution adopted at a legally convened meeting of the Space Coast Transportation Planning Organization Governing Board.

By: 
 Andrea Young
 Space Coast TPO Governing Board Chair



By: 
 Jerry Allender
 Space Coast TPO Governing Board Secretary

Space Coast TPO - Advance 2050 LRTP -
adopted 9/11/2025

SCTPO | Advance 2050 Long Range Transportation Plan

Table 4-3. Cost Feasible Plan | Regionally Significant – State Road Projects

Project #	Roadway	Limits	Project Scores/Rankings		Implementation Timeframe (Year of Expenditure \$)	Funding Needed to Complete Project (Present Day \$)
			Total Score (0-100)	LoPP Rank		
Fully Funded						
1	SR 519 (Fiske Blvd) at Roy Wall Blvd Intersection Improvements with Crosswalks		87	1 RS	2031-2035 \$4,795,000 (State)	Not Applicable
2	SR 524 London Blvd to SR 524 (Industry Rd) Widen to 4 Lanes with Trail		86	2 RS	2036-2040 \$13,686,071 (State)	Not Applicable
3	SR 501 (Clearlake Rd) Michigan Ave to SR 524 (Industry Rd) Widen to 4 Lanes with Roundabout & Sidewalk		85	3 RS	2041-2045 \$2,219,400 is ROW \$34,508,000 (State) \$11,466,671 is CST \$30,912,000 (Federal)	Not Applicable
4	SR A1A Long Point Rd to George King Blvd Context Based Design Improvements with Curb & Gutter / Median		84	4 RS	2046-2050 \$21,630,000 (State) \$74,427,800 (Federal)	Not Applicable
7	US 1 at Viera Blvd Intersection Improvements with Crosswalks		60	9 RS	2031-2035 \$1,378,654 (Federal)	Not Applicable
8	SR 518 (Eou Gallie Blvd) at Wickham Rd Intersection Improvements with Crosswalks		56	10 RS	2036-2040 \$17,726,239 (Federal)	Not Applicable
Partially Funded						
2	SR 524 at London Blvd Roundabout with Crosswalks		86	2 RS	Partially Funded (PE) 2026-2030 \$7,402,342 – in TIP	CST \$949,865
2	SR 524 Cox Rd to London Blvd Widen to 4 Lanes with Trail		86	2 RS	Partially Funded (ROW) 2031-2035 \$4,718,965 (State)	CST \$15,151,180
2	SR 524 at Cox Rd Roundabout with Crosswalks		86	2 RS	Partially Funded (PE) 2026-2030 \$7,402,342 – in TIP	CST \$8,706,108
2	SR 524 Friday Rd to Cox Rd Widen to 4 Lanes with Trail		86	2 RS	Partially Funded (ROW) 2031-2035 \$4,523,265 (State)	CST \$14,527,273
5	SR 405 (South St) SR 50 (Cheney Hwy) to US 1 Widen to 4 Lanes with Multimodal Improvements (TBD)		81	5 RS	Partially Funded (PE) 2031-2035 \$29,085,000 (State)	ROW, CST \$85,393,158
6	SR 514 (Malabar Rd) SR 507 (Babcock St) to US 1 Widen to 4 Lanes with Trail		76	6 RS	Partially Funded (PE) 2031-2035 \$19,440,000 (State)	ROW, CST \$57,088,875

PE Funding:
\$7,402,342 - Funded in prior years
ROW Funding:
\$2,219,400 - FY 2036-2040 (London to Industry)
\$4,718,965 - FY 2031-2035 (Cox to London)
\$4,523,265 - FY 2031-2035 (Friday to Cox)
\$11,461,630 - Total ROW
(\$9,242,230 FY 31-35 and \$2,219,400 FY 36-40)
CST Funding
\$11,466,671 - FY 2036-2040 (London to Industry)
Unfunded CST
\$949,865 + \$15,151,180 + \$8,706,708 + \$14,527,273
\$39,334,426 - Total Unfunded CST



Space Coast Transportation Planning Organization

RESOLUTION # 24-10

2045 Long Range Transportation Plan Amendment No. 5

A RESOLUTION, adopting the Space Coast Transportation Planning Organization’s 2045 Long Range Transportation Plan (LRTP) for the Palm Bay-Melbourne and Titusville Urbanized Areas.

WHEREAS, the Space Coast Transportation Planning Organization is the designated and constituted body responsible for the urban transportation planning and programming process for the Palm Bay-Melbourne and Titusville Urbanized Areas; and

WHEREAS, Federal regulations 23 C.F.R. 450.322, 450.324, 450.306 and FL State Statute 339.175 outline the requirements for MPOs to develop LRTPs through a performance-driven, outcome-based approach to planning for metropolitan areas of the State. The metropolitan transportation planning process shall be continuous, cooperative, and comprehensive; it should also provide for the consideration and implementation of projects, strategies, and services that will address all required factors; and

WHEREAS, the Long Range Transportation Plan (LRTP) was developed with local, regional, state, multi-modal and environmental agencies that depicts an accurate representation of the areas priorities as developed through the planning process carried on cooperatively in accordance with the provisions of 23 U.S.C. 134; and

WHEREAS, waiting for the next regularly scheduled Governing Board meeting for approval will cause unnecessary delay in FDOT operations; and

WHEREAS, this Amendment is being approved in compliance with Policy PLC-3, Budget and Finance Section 7.0(C)(1) SCTPO Executive Director Approval Authority, to ensure planning consistency with the SCTPO LRTP; and

WHEREAS, this Amendment will be presented to the full TPO Governing Board at their next regularly scheduled meeting.

NOW THEREFORE, BE IT RESOLVED, The Space Coast Transportation Planning Organization amends the 2045 Long Range Transportation Plan, Amendment No. 5, as detailed in Attachment “A”.




Space Coast Transportation Planning Organization

Passed and duly adopted by the Space Coast Transportation Planning Organization Governing Board's Chair on the 15th day of December, 2023.

Certificate

The undersigned duly qualified as Chair of the Space Coast Transportation Planning Organization Governing Board certifies that the foregoing is a true and correct copy of a Resolution adopted at a legally convened meeting of the Space Coast Transportation Planning Organization Governing Board.

By: Andrea Young
 Andrea Young
 Space Coast TPO Governing Board Chair



By: Georganna Gillette
 Georganna Gillette
 Space Coast TPO Executive Director

Space Coast TPO - LRTP
Amendment #5 - adopted 12/15/23

Project ID	Facility	2021-2025		Source	2022-2030		2031-2035		2036-2040		2041-2045		Total
		PD&E	ROW		PD&E	ROW	PD&E	ROW	PD&E	ROW	PD&E	ROW	
T2.2	SR 405 (Garden St.) at Singleton Ave.	US 1	Widened to 4 Lanes	Other Arterial									\$ 3.35
T2.3	Space Coast Trail	N/A	Operational Analysis	Other Arterial									\$ 7.31
T2.4	FL Coast to Coast Parrish Park Trailhead	N/A	Bike Path/Trial	DDR, DDI	\$ 6.84								\$ 6.84
T2.5	SR 405 (South St.)	SR 50	Widened to 4 Lanes	DHI, PLH	\$ 1.86								\$ 1.86
T2.6	SR 524 S Friday Rd.		Widened to 4 Lanes	Other Arterial									\$ 2.02
T2.7	SR 501 (Clearlake Rd.)		Widened to 4 Lanes	TMA (SU)	\$ 0.40								\$ 50.34
T2.8	SR 520 Orange County Line		Widened to 4 Lanes	ACSU, CARU	\$ 1.39								\$ 38.28
T2.9	SR 520 Aurora Rd.		Resurficing	TALU	\$ 0.61								\$ 48.31
T2.10	SR 520 Lake Dr.		Safety Project	Local Funds	\$ 5.00								\$ 49.47
T2.11	SR 520 E of Millard Point Dr.		Resurficing	Other Arterial									\$ 4.81
T2.12	SR 519/Fiske Blvd.		Resurficing	DDR, DDI, DS	\$ 4.81								\$ 8.87
T2.13	SR 37A Courtenay Pkwy. at Mustang Way		Misc. Construction	TMA (SU)	\$ 1.78								\$ 1.78
T2.14	SR 37A Courtenay Pkwy.		Resurficing	ACSS, DDR	\$ 1.08								\$ 5.68
T2.15	SR 518 at N Atlantic Ave./International Dr.		Misc. Construction	DDR, DDI	\$ 3.95								\$ 9.27
T2.16	SR 518 at Wickham Rd.		Resurficing	DDR, DDI, DS	\$ 9.27								\$ 8.60
T2.17	SR 518 at Wickham Rd.		Misc. Construction	ACSS, DDR	\$ 0.77								\$ 1.61
T2.18	SR 518 at Wickham Rd.		Resurficing	DDR, DDI, SA	\$ 9.11								\$ 9.11
T2.19	SR 518 at Wickham Rd.		Misc. Construction	TMA (SU)	\$ 2.64								\$ 2.64
T2.20	SR 518 at Wickham Rd.		Misc. Construction	Other Arterial	\$ 5.40								\$ 5.40
T2.21	SR 518 at Wickham Rd.		Misc. Construction	Other Arterial	\$ 18.00								\$ 30.45
T2.22	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 0.55								\$ 0.55
T2.23	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 1.11								\$ 1.32
T2.24	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 0.21								\$ 3.51
T2.25	SR 518 at Wickham Rd.		Misc. Construction	TMA (SU)	\$ 0.81								\$ 0.81
T2.26	SR 518 at Wickham Rd.		Misc. Construction	TALU	\$ 0.61								\$ 0.61
T2.27	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 0.26								\$ 1.48
T2.28	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 1.22								\$ 2.83
T2.29	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 2.83								\$ 4.95
T2.30	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 4.95								\$ 13.58
T2.31	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 0.54								\$ 0.54
T2.32	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 0.71								\$ 0.71
T2.33	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 0.67								\$ 6.60
T2.34	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 0.51								\$ 1.18
T2.35	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 0.35								\$ 0.35
T2.36	SR 518 at Wickham Rd.		Misc. Construction	TMA (SU)	\$ 0.30								\$ 0.30
T2.37	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 0.30								\$ 0.30
T2.38	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 1.29								\$ 1.29
T2.39	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 12.79								\$ 12.79
T2.40	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 0.52								\$ 0.52
T2.41	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 0.72								\$ 23.57
T2.42	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 0.34								\$ 16.88
T2.43	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 0.01								\$ 0.01
T2.44	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 1.82								\$ 1.82
T2.45	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 1.04								\$ 4.38
T2.46	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 2.13								\$ 2.13
T2.47	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 0.23								\$ 0.23
T2.48	SR 518 at Wickham Rd.		Misc. Construction	ACSS	\$ 0.50								\$ 0.56

\$7,400,000

Space Coast Transportation Planning Organization Transportation Improvement Program FY 25 - 29

Adopted by the Space Coast TPO Board on July 12, 2024

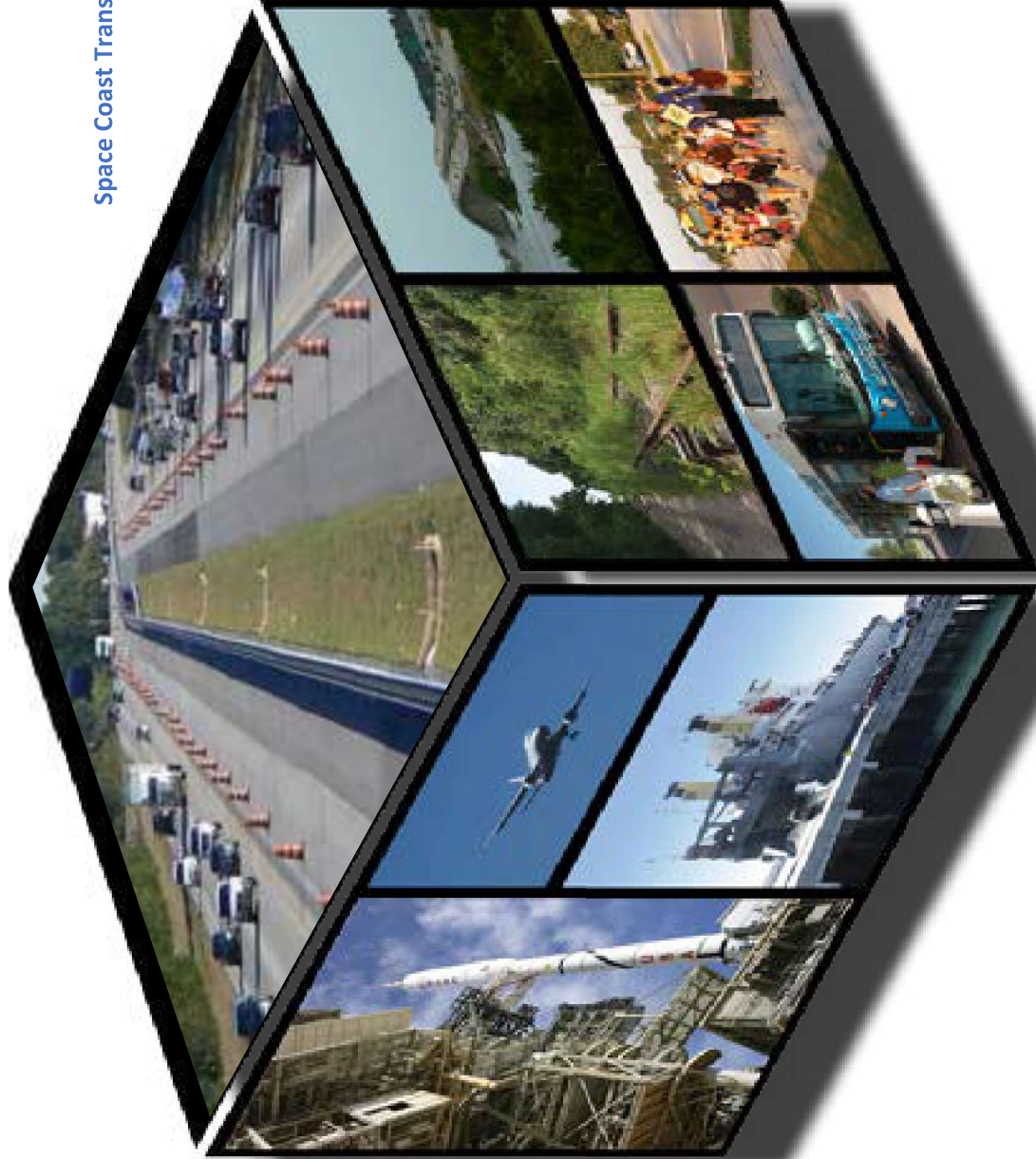
Roll Forward Amendment Sept. 12, 2024, Emergency TIP Amendment October 8, 2024

Space Coast TPO - 2025-29 TIP
Amended 10/8/24

Since PE funding was in FY2025, project
does not appear in 2026-30 TIP



Space Coast Transportation Planning Organization
2725 Judge Fran Jamieson Way
Building B, Room 105
Melbourne, Florida 32940
Phone: 321-690-6890
Fax: 321-690-6827



The preparation of this report has been financed in part through grant(s) from the Federal Highway Administration and Federal Transit Administration, U.S. Department of Transportation, under the State Planning and Research Program, Section 505 [or Metropolitan Planning Program, Section 104(f)] of Title 23, U.S. Code. The contents of this report do not necessarily reflect the official views or policy of the U.S. Department of Transportation.

Space Coast TPO - 2025-29 TIP
 Amended 10/8/24
 Since PE funding was in FY2025, project
 does not appear in 2026-30 TIP

Space Coast TPO Transportation Improvement Program - FY 2025 - 2029

Section A - Highway & Bridge Capacity

Phase	Fund Source	2025	2026	2027	2028	2029	Total
Proj# 4372101 MALABAR RD FROM ST JOHNS HERITAGE PKWY TO MINTON RD Length: 3.97 MI *Non-SIS* Lead Agency: MANAGED BY CITY OF PALM BAY LRTP #: Page 215 & 216							
Description: Off State Highway System							
PE	LF	7,000,000	0	0	0	0	7,000,000
PE	ACSU	1,942,634	0	0	0	0	1,942,634
PE	TALU	476,000	0	0	0	0	476,000
PE	CARU	292,012	0	0	0	0	292,012
PE	SU	180,969	0	0	0	0	180,969
PE	TALT	147,385	0	0	0	0	147,385
Total		10,039,000	0	0	0	0	10,039,000
		Prior Years Cost	Future Years Cost			Total Project Cost	11,413,667
Proj# 4379831 SR 524 FROM FRIDAY ROAD TO INDUSTRY ROAD Length: 3.141 MI *Non-SIS* Lead Agency: MANAGED BY FDOT LRTP #: Page 171							
Description: Widening 2 to 4 lanes							
PE	LF	5,000,000	0	0	0	0	5,000,000
PE	CARU	990,018	0	0	0	0	990,018
PE	TALU	608,949	0	0	0	0	608,949
PE	SU	404,320	0	0	0	0	404,320
PE	ACSU	399,055	0	0	0	0	399,055
Total		7,402,342	0	0	0	0	7,402,342
		Prior Years Cost	Future Years Cost			Total Project Cost	9,461,255
Proj# 4404241 NASA CAUSEWAY BRIDGE Length: 3.593 MI *SIS* Lead Agency: MANAGED BY FDOT LRTP #: Page 142							
Description: BRIDGE REPLACEMENT							
INC	GMR	2,000,000	0	0	0	0	2,000,000
Total		2,000,000	0	0	0	0	2,000,000
		Prior Years Cost	Future Years Cost			Total Project Cost	150,310,746



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Web Application

Federal Aid Management Sabrina Aubery - Manager

STIP Project Detail and Summaries Online Report

**** Repayment Phases are not included in the Totals ****

Selection Criteria	
Approved STIP Financial Project: 437983 As Of: 7/1/2025	Detail Related Items Shown

HIGHWAYS							
Item Number: 437983 1		Project Description: SR 524 FROM FRIDAY ROAD TO INDUSTRY ROAD					
District: 05	County: BREVARD	Type of Work: PD&E/EMO STUDY			Project Length: 3.141MI		
		Fiscal Year					
Phase / Responsible Agency	<2026	2026	2027	2028	2029	>2029	All Years
PLANNING / MANAGED BY FDOT							
Fund Code: DS-STATE PRIMARY HIGHWAYS & PTO	181,559						181,559
P D & E / MANAGED BY FDOT							
Fund Code: DDR-DISTRICT DEDICATED REVENUE	733,264						733,264
DIH-STATE IN-HOUSE PRODUCT SUPPORT	95,496	4,096					99,592
DS-STATE PRIMARY HIGHWAYS & PTO	153,584						153,584
SU-STP, URBAN AREAS > 200K	943,436						943,436
Phase: P D & E Totals	1,925,780	4,096					1,929,876
PRELIMINARY ENGINEERING / MANAGED BY FDOT							
Fund Code: ACSU-ADVANCE CONSTRUCTION (SU)	808,375						808,375
ARPI-ARPA INTEREST	991,375						991,375

DIH-STATE IN-HOUSE PRODUCT SUPPORT	5,515	5,485						11,000
DS-STATE PRIMARY HIGHWAYS & PTO	13,150							13,150
LF-LOCAL FUNDS		4,289,942						4,289,942
SU-STP, URBAN AREAS > 200K	177,788							177,788
TALU-TRANSPORTATION ALTS->200K	1,134,862							1,134,862
Phase: PRELIMINARY ENGINEERING Totals	3,131,065	4,295,427						7,426,492
Item: 437983 1 Totals	5,238,404	4,299,523						9,537,927
Project Totals	5,238,404	4,299,523						9,537,927
Grand Total	5,238,404	4,299,523						9,537,927

This site is maintained by the Office of Work Program and Budget, located at 605 Suwannee Street, MS 21, Tallahassee, Florida 32399.

For additional information please e-mail questions or comments to:
Federal Aid Management
Sabrina Aubery: Sabrina.Aubery@dot.state.fl.us Or call 850-414-4449
Or
Dawn Rudolph: Dawn.Rudolph@dot.state.fl.us Or call 850-414-4465

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Florida Department of Transportation

Consistent, Predictable, Repeatable

Community Effects Appendix

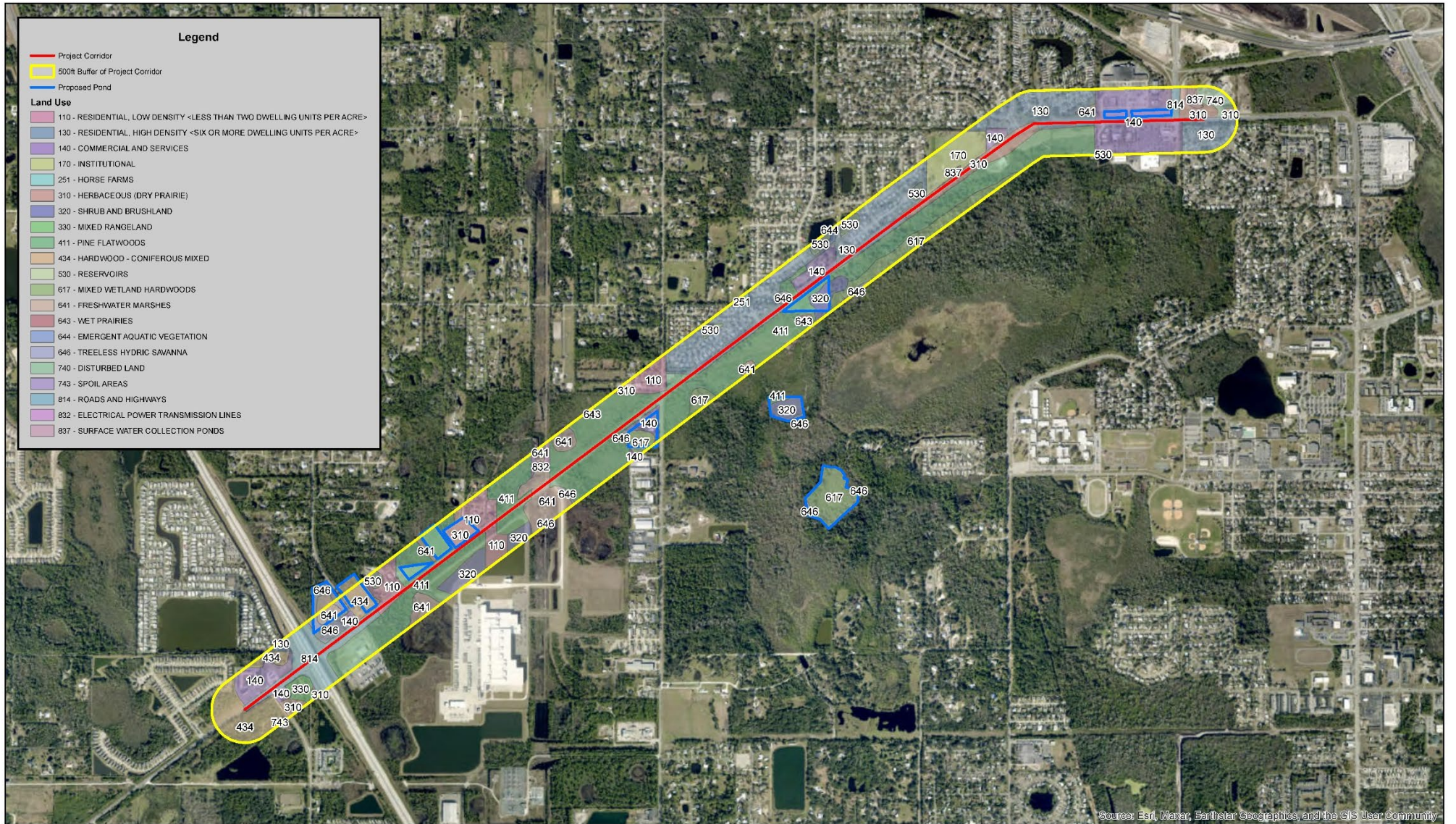
Contents:

Figure 3-1: Existing Land Use Map

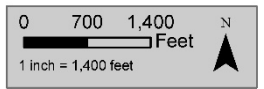
Figure 3-2: Future Land Use Map - City of Cocoa

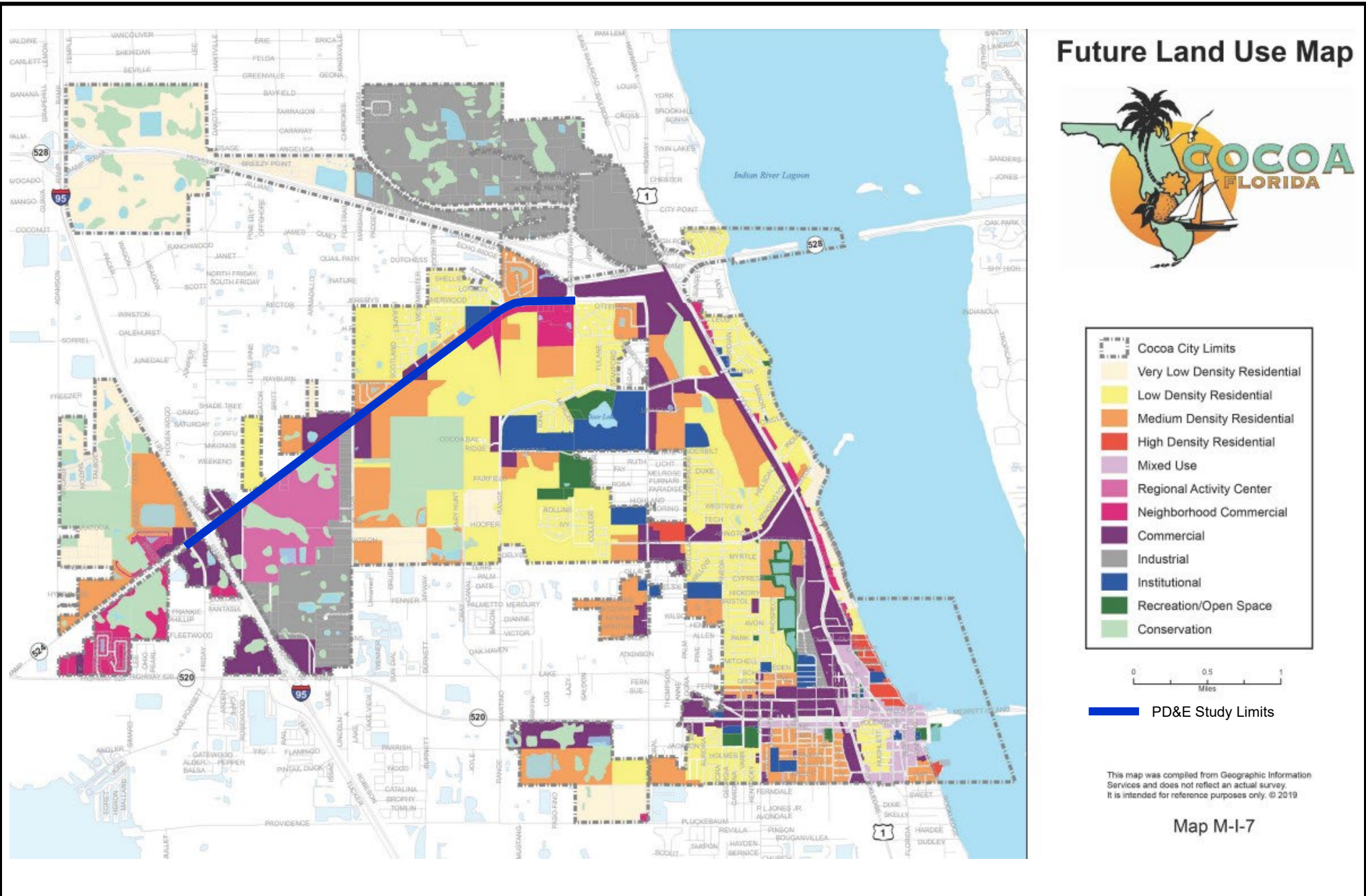
Figure 3-3: Future Land Use Map - Brevard County

Farmland Conversion Impact Rating Form (December 2024)



REFERENCE: THE 2018 AERIAL PHOTOGRAPH WAS OBTAINED FROM ESRI. THE LAND USE DATA WAS OBTAINED FROM THE SJRWMD. THE PRESENTED DATA IS FOR INFORMATIONAL PURPOSES ONLY. IT IS NOT MEANT FOR DESIGN, LEGAL, OR ANY OTHER USES. PSI ASSUMES NO RESPONSIBILITY FOR ANY DECISIONS MADE OR ANY ACTIONS TAKEN BY THE USER BASED UPON INFORMATION OBTAINED FROM THE ABOVE DATA.

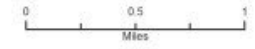




Future Land Use Map



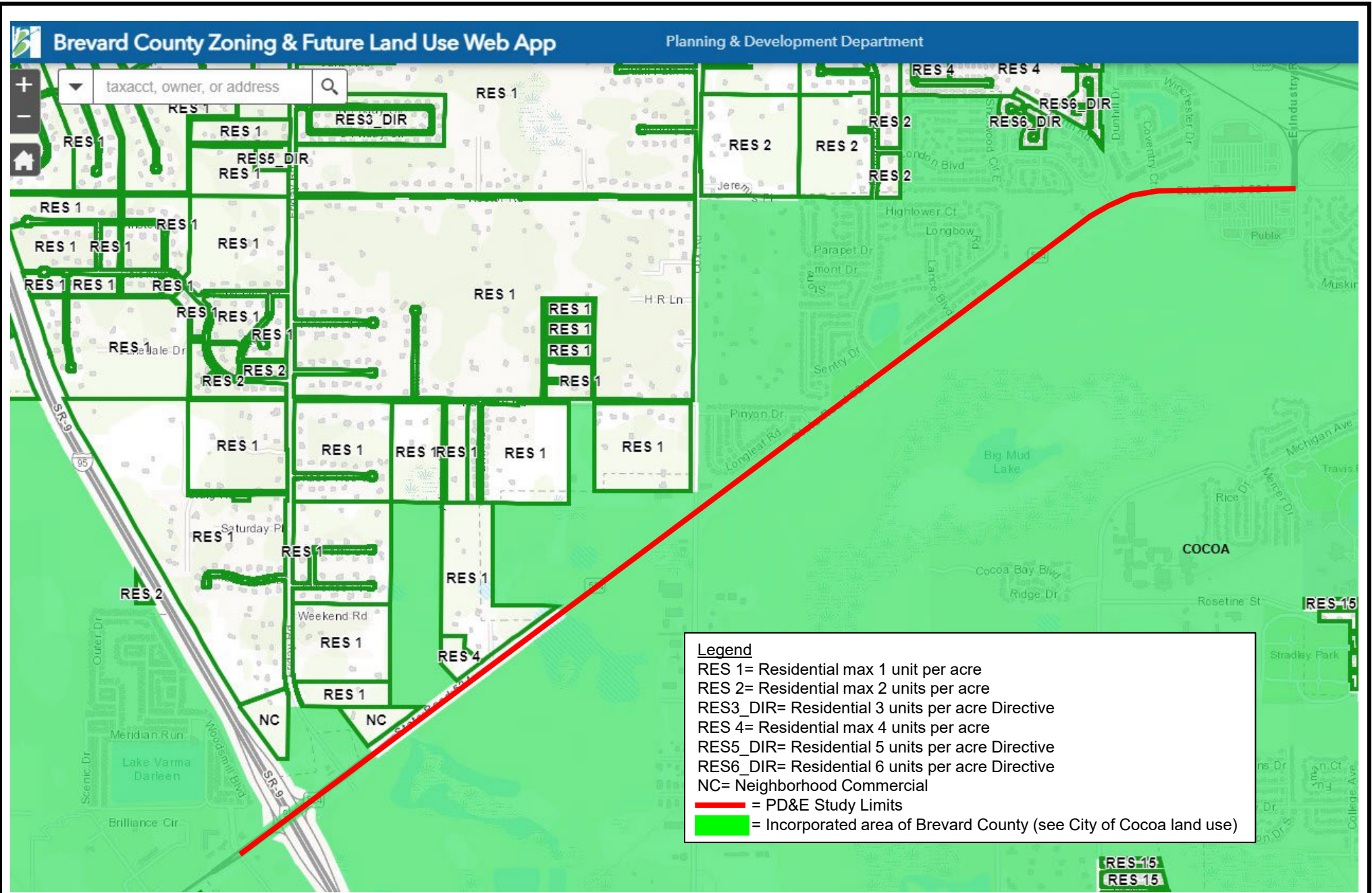
- Cocoa City Limits
- Very Low Density Residential
- Low Density Residential
- Medium Density Residential
- High Density Residential
- Mixed Use
- Regional Activity Center
- Neighborhood Commercial
- Commercial
- Industrial
- Institutional
- Recreation/Open Space
- Conservation



PD&E Study Limits

This map was compiled from Geographic Information Services and does not reflect an actual survey. It is intended for reference purposes only. © 2019

Map M-I-7



**FARMLAND CONVERSION IMPACT RATING
FOR CORRIDOR TYPE PROJECTS**

PART I (To be completed by Federal Agency)		3. Date of Land Evaluation Request 12/9/24	4. Sheet 1 of 2
1. Name of Project SR 524 PD&E; FM 437983-1-22-01		5. Federal Agency Involved FDOT	
2. Type of Project Two-lane to four-lane widening of 3.15 mile road		6. County and State Brevard, County, Florida	
PART II (To be completed by NRCS)		1. Date Request Received by NRCS 12/16/24	2. Person Completing Form Josue Aceituno
3. Does the corridor contain prime, unique statewide or local important farmland? (If no, the FPPA does not apply - Do not complete additional parts of this form). YES <input checked="" type="checkbox"/> NO <input type="checkbox"/>		4. Acres Irrigated 19,292	Average Farm Size 249
5. Major Crop(s) Sod; Forage	6. Farmable Land in Government Jurisdiction Acres: 38,351 % 5.89	7. Amount of Farmland As Defined in FPPA Acres: 11,950 % 0.02	
8. Name Of Land Evaluation System Used None	9. Name of Local Site Assessment System Soil Potential Rating	10. Date Land Evaluation Returned by NRCS 12/16/24	

PART III (To be completed by Federal Agency)	Alternative Corridor For Segment			
	Corridor A	Corridor B	Corridor C	Corridor D
A. Total Acres To Be Converted Directly	0	0	0	
B. Total Acres To Be Converted Indirectly, Or To Receive Services	0	0	0	
C. Total Acres In Corridor	116.37	115.45	119.15	

PART IV (To be completed by NRCS) Land Evaluation Information	Corridor A	Corridor B	Corridor C	Corridor D
A. Total Acres Prime And Unique Farmland	48	48	48	
B. Total Acres Statewide And Local Important Farmland	0	0	0	
C. Percentage Of Farmland in County Or Local Govt. Unit To Be Converted	0.13	0.13	0.13	
D. Percentage Of Farmland in Govt. Jurisdiction With Same Or Higher Relative Value	24.1	24.1	24.1	


PART V (To be completed by NRCS) Land Evaluation Information Criterion Relative value of Farmland to Be Serviced or Converted (Scale of 0 - 100 Points)	Corridor A	Corridor B	Corridor C	Corridor D
	48.4	48.4	48.4	

PART VI (To be completed by Federal Agency) Corridor Assessment Criteria (These criteria are explained in 7 CFR 658.5(c))	Maximum Points	Corridor A	Corridor B	Corridor C	Corridor D
1. Area in Nonurban Use	15	1	1	1	
2. Perimeter in Nonurban Use	10	0	0	0	
3. Percent Of Corridor Being Farmed	20	0	0	0	
4. Protection Provided By State And Local Government	20	0	0	0	
5. Size of Present Farm Unit Compared To Average	10	0	0	0	
6. Creation Of Nonfarmable Farmland	25	0	0	0	
7. Availability Of Farm Support Services	5	3	3	3	
8. On-Farm Investments	20	0	0	0	
9. Effects Of Conversion On Farm Support Services	25	0	0	0	
10. Compatibility With Existing Agricultural Use	10	0	0	0	
TOTAL CORRIDOR ASSESSMENT POINTS	160	4	4	4	0

PART VII (To be completed by Federal Agency)	Corridor A	Corridor B	Corridor C	Corridor D
Relative Value Of Farmland (From Part V)	100	48.4	48.4	48.4
Total Corridor Assessment (From Part VI above or a local site assessment)	160	4	4	4
TOTAL POINTS (Total of above 2 lines)	260	52.4	52.4	52.4

1. Corridor Selected: A	2. Total Acres of Farmlands to be Converted by Project: 48.4	3. Date Of Selection: 5/12/2023	4. Was A Local Site Assessment Used? YES <input checked="" type="checkbox"/> NO <input type="checkbox"/>
-----------------------------------	--	---	---

5. Reason For Selection:
Corridor A was selected because it met the drainage requirements for the project, minimized the need for new right-of-way and avoided cultural resource and environmental impacts associated with the other two corridors. The primary difference between the corridors were the pond alternatives.

Signature of Person Completing this Part:  DATE **12/18/2024**

NOTE: Complete a form for each segment with more than one Alternate Corridor

CORRIDOR - TYPE SITE ASSESSMENT CRITERIA

The following criteria are to be used for projects that have a linear or corridor - type site configuration connecting two distant points, and crossing several different tracts of land. These include utility lines, highways, railroads, stream improvements, and flood control systems. Federal agencies are to assess the suitability of each corridor - type site or design alternative for protection as farmland along with the land evaluation information.

(1) How much land is in nonurban use within a radius of 1.0 mile from where the project is intended?

More than 90 percent - 15 points
 90 to 20 percent - 14 to 1 point(s)
 Less than 20 percent - 0 points

(2) How much of the perimeter of the site borders on land in nonurban use?

More than 90 percent - 10 points
 90 to 20 percent - 9 to 1 point(s)
 Less than 20 percent - 0 points

(3) How much of the site has been farmed (managed for a scheduled harvest or timber activity) more than five of the last 10 years?

More than 90 percent - 20 points
 90 to 20 percent - 19 to 1 point(s)
 Less than 20 percent - 0 points

(4) Is the site subject to state or unit of local government policies or programs to protect farmland or covered by private programs to protect farmland?

Site is protected - 20 points
 Site is not protected - 0 points

(5) Is the farm unit(s) containing the site (before the project) as large as the average - size farming unit in the County?

(Average farm sizes in each county are available from the NRCS field offices in each state. Data are from the latest available Census of Agriculture, Acreage or Farm Units in Operation with \$1,000 or more in sales.)
 As large or larger - 10 points
 Below average - deduct 1 point for each 5 percent below the average, down to 0 points if 50 percent or more below average - 9 to 0 points

(6) If the site is chosen for the project, how much of the remaining land on the farm will become non-farmable because of interference with land patterns?

Acreage equal to more than 25 percent of acres directly converted by the project - 25 points
 Acreage equal to between 25 and 5 percent of the acres directly converted by the project - 1 to 24 point(s)
 Acreage equal to less than 5 percent of the acres directly converted by the project - 0 points

(7) Does the site have available adequate supply of farm support services and markets, i.e., farm suppliers, equipment dealers, processing and storage facilities and farmer's markets?

All required services are available - 5 points
 Some required services are available - 4 to 1 point(s)
 No required services are available - 0 points

(8) Does the site have substantial and well-maintained on-farm investments such as barns, other storage building, fruit trees and vines, field terraces, drainage, irrigation, waterways, or other soil and water conservation measures?

High amount of on-farm investment - 20 points
 Moderate amount of on-farm investment - 19 to 1 point(s)
 No on-farm investment - 0 points

(9) Would the project at this site, by converting farmland to nonagricultural use, reduce the demand for farm support services so as to jeopardize the continued existence of these support services and thus, the viability of the farms remaining in the area?

Substantial reduction in demand for support services if the site is converted - 25 points
 Some reduction in demand for support services if the site is converted - 1 to 24 point(s)
 No significant reduction in demand for support services if the site is converted - 0 points

(10) Is the kind and intensity of the proposed use of the site sufficiently incompatible with agriculture that it is likely to contribute to the eventual conversion of surrounding farmland to nonagricultural use?

Proposed project is incompatible to existing agricultural use of surrounding farmland - 10 points
 Proposed project is tolerable to existing agricultural use of surrounding farmland - 9 to 1 point(s)
 Proposed project is fully compatible with existing agricultural use of surrounding farmland - 0 points

Cultural Resources Appendix

Contents:

- SHPO Concurrence Letter (October 2020)
- Seminole Tribe of FL Response (April 2024)
- Section 4(f) Report



Florida Department of Transportation

RON DESANTIS
GOVERNOR

719 S. Woodland Blvd.
DeLand, FL 32720

KEVIN J. THIBAUT, P.E.
SECRETARY

September 15, 2020

Timothy A. Parsons, Ph.D.,
Director and State Historic Preservation Officer
Florida Division of Historical Resources
Florida Department of State
R.A. Gray Building
500 South Bronough Street
Tallahassee, Florida 32399-0250

Attn: Dr. Adrienne Daggett, Transportation Compliance Review Program

RE: Cultural Resource Assessment Survey (revised)
State Road 524 Improvements
Project Development and Environment (PD&E) Study
North Friday Road to Industry Road
Brevard County, Florida
FPID No.: 437983-1-22-01

Dear Dr. Parsons,

Enclosed please find one copy of the revised report titled *Cultural Resource Assessment Survey for the State Road 524 Improvements Project Development and Environment Study from North Friday Road to Industry Road, Brevard County, Florida*. This report presents the findings of a Phase I cultural resource assessment survey (CRAS) conducted in support of a Project Development and Environment (PD&E) study for improvements of State Road (SR) 524 in Brevard County, Florida. The Florida Department of Transportation (FDOT), District 5, is investigating the expansion of the two-lane rural roadway to a four-lane divided facility to increase the capacity of SR 524 along an approximately 3.4-mile (5.5-kilometer) segment from North Friday Road northeast to Industry Road. The project also will improve safety, provide multi-modal facilities for pedestrian and bicyclists, and evaluate improvements to the Interstate 95 (I-95) interchange. Additionally, 14 potential pond locations were surveyed in conjunction with the SR 524 improvements.

The project Area of Potential Effects (APE) for the roadway corridor was defined as the maximum SR 524 right-of-way incorporating all potential alternatives, extended to the back or side property lines of parcels adjacent to proposed new right-of-way, or a distance of no more than 328 feet (100 meters) from the maximum right-of-way line. For the proposed ponds, the APE was defined as the pond footprint with an additional 100-foot (30.5-meter) buffer. The archaeological survey was conducted within the existing and proposed right-of-way, as well as

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Dr. Parsons, SHPO
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September 15, 2020
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within the pond footprints. The historic structure survey was conducted within the entire SR 524 Improvements APE and SR 524 Improvements Ponds APE.

This CRAS was conducted to comply with Chapter 267 of the Florida Statutes and Rule Chapter 1A-46, Florida Administrative Code. All work was performed in accordance with Part 2, Chapter 8 of the FDOT's PD&E Manual (revised July 2020), as well as the Florida Division of Historical Resources' (FDHR) recommendations for such projects, as stipulated in the FDHR's *Cultural Resource Management Standards & Operations Manual, Module Three: Guidelines for Use by Historic Preservation Professionals*. The Principal Investigator for this project meets the Secretary of the Interior's *Standards and Guidelines for Archeology and Historic Preservation* (48 FR 44716-42). This study complies with Public Law 113-287 (Title 54 U.S.C.), which incorporates the provisions of the National Historic Preservation Act (NHPA) of 1966, as amended, and the Archeological and Historic Preservation Act of 1979, as amended. The study also complies with the regulations for implementing NHPA Section 106 found in 36 CFR Part 800 (*Protection of Historic Properties*).

The archaeological field survey included visual reconnaissance and intensive systematic subsurface examination of the project right-of-way and pond footprints. A total of 75 shovel tests were excavated, with three shovel tests positive for cultural material, resulting in the identification of one newly recorded prehistoric archaeological site, Cocoa Hill (8BR04221). This site was identified along a proposed access road to Ponds 2B and 2A; the narrow footprint limited delineation of the site to a single transect of shovel tests. As testing outside of the right-of-way is beyond the scope of this project, insufficient information is available to evaluate 8BR04221 for listing on the National Register of Historic Places (NRHP). Although the density of prehistoric cultural materials was moderate to high, the nature of the artifact assemblage is unexceptional. Considering the limited artifact assemblage, and the lack of diagnostic artifacts and subsurface features, it does not appear that the Cocoa Hill site (8BR04221), as expressed within the current project limits, has the potential to yield further information important in the prehistory of the region. In the opinion of SEARCH, insufficient information is available to evaluate 8BR04221 in its entirety; however, the portion of 8BR04221 located within the SR 524 Improvements project limits is recommended ineligible for the NRHP.

A discussion was initiated with the Project Engineer regarding the proposed work in the vicinity of the newly-recorded archaeological site. First, Ponds 2A and 2B, proposed along the access road where Cocoa Hill (8BR04221) is located, are only two of seven ponds (2A-2F) associated with the drainage area defined as "Basin 2." Second, for Basin 2 (Ponds 2A-2F), it is anticipated that only two of the seven proposed ponds will be selected and constructed (one for stormwater management and the other for floodplain compensation). Third, Pond 2A is the least favorable pond due to other contributing factors such as location, hydraulics, and conservation. The access road would only need to be extended through the newly recorded site should Pond 2A be selected.

In terms of the access road, it is proposed to be 12-foot-wide within a 30-foot drainage easement. The access road will be mostly in-fill with a 12-inch stabilized (no asphalt) base. The

Dr. Parsons, SHPO
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September 15, 2020
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stabilization will not be cut into the ground as it will be part of the fill material. Also, should Pond 2B be selected and not Pond 2A, the access road will end before reaching the boundary of the Cocoa Hill (8BR04221) site.

As discussed above, shovel testing revealed a moderate to high density of prehistoric cultural materials within the recorded boundary of the Cocoa Hill (8BR04221) site but the nature of the artifact assemblage is unexceptional exhibiting a lack of stone tools, diagnostic artifacts, and subsurface features. Due to the limits of delineation, the density of recovered artifacts, and the potential for the site to extend beyond the limits of the archaeological APE, if additional work is proposed outside of the current right-of-way and pond footprints within 100 meters of 8BR04221, additional archaeological survey will be required.

No other sites or occurrences were identified as part of the archaeological survey. No further archaeological work is recommended for the SR 524 Improvements project.

The architectural survey resulted in the identification and evaluation of six historic resources within the SR 524 Improvements APE, including one previously recorded resource and five newly recorded resources. SEARCH recommends that the six historic resources (8BR03331, 8BR04195-8BR4198, and 8BR04214) are ineligible for the NRHP due to a lack of the significant historic associations and architectural distinction. No further architectural work is recommended.

Based on the results of this study, it is the opinion of the District that the proposed undertaking will have no effect on NRHP-listed or -eligible historic properties. No further work is recommended.

I respectfully request your concurrence with the findings of the enclosed report.

If you have any questions or need further assistance, please contact Catherine Owen, District Cultural Resource Coordinator, at (386) 943-5383 or me at (386) 943-5411.

Sincerely,




William G. Walsh
Environmental Manager
FDOT, District Five

Dr. Parsons, SHPO
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September 15, 2020
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The Florida Division of Historical Resources finds the attached Cultural Resource Assessment Report complete and sufficient and concurs / does not concur with the determinations of historic significance provided in this cover letter and does / does not find applicable the determinations of effects provided in this cover letter for SHPO/FDHR Project File Number 2020-4428B.
Conditional upon the following:

FDHR Comments: If pond 2B is selected, we request additional close-interval testing in the south-east corner of the pond site to determine whether site BR4221 extends into that area.

 _____ Timothy A. Parsons, PhD, Director Florida Division of Historical Resources	<u>October 2, 2020</u> _____ Date
---	---

From: Micheline Hilpert <michelinehilpert@semtribe.com>
Sent: Monday, April 29, 2024 3:23 PM
To: Owen, Catherine <Catherine.Owen@dot.state.fl.us>; THPO Compliance <THPOCompliance@semtribe.com>
Cc: Rothrock, Lindsay <Lindsay.Rothrock@dot.state.fl.us>
Subject: RE: FM# 437983-1 SR 524 from North Friday Road to Industry Road, Brevard County - PD&E Study CRAS

EXTERNAL SENDER: Use caution with links and attachments.

SEMINOLE TRIBE OF FLORIDA
TRIBAL HISTORIC PRESERVATION OFFICE

TRIBAL HISTORIC
PRESERVATION OFFICE

SEMINOLE TRIBE OF FLORIDA

30290 JOSIE BILLIE HIGHWAY
PMB 1004
CLEWISTON, FL 33440

THPO PHONE: (863) 983-6549
FAX: (863) 902-1117

THPO WEBSITE: WWW.STOFTHPO.COM



TRIBAL OFFICERS

MARCELLUS W. OSCEOLA JR.
CHAIRMAN

MITCHELL GYPRESS
VICE CHAIRMAN

LAVONNE ROSE
SECRETARY

PETER A. HAHN
TREASURER

April 29, 2024

Catherine B. Owen, M.S.
District Cultural Resources Coordinator
FDOT District Five
719 S. Woodland Blvd.
DeLand, FL 32720
Email: Catherine.Owen@dot.state.fl.us
Phone: 386-943-5383

Subject: FM# 437983-1 SR 524 from North Friday Road to Industry Road, Brevard County, Florida
THPO Compliance Tracking Number: 0034395

In order to expedite the THPO review process:

1. Please correspond via email and provide documents as attachments,
2. Please send all emails to THPOCompliance@semtribe.com,
3. Please reference the THPO Compliance Tracking Number if one has been assigned.

Dear Catherin Owen,

Thank you for contacting the Seminole Tribe of Florida Tribal Historic Preservation Office (STOF THPO) Compliance Section regarding the *FM# 437983-1 SR 524 from North Friday Road to Industry Road, Brevard County, Florida*.

The proposed undertaking does fall within the STOF Area of Interest. We have reviewed the documents that you provided and completed our assessment pursuant to Section 106 of the National Historic Preservation Act (16 USC 470) as amended and its implementing regulations (36 CFR 800). In response, our office would like to provide the following comments:

- We acknowledge that the portion of the archaeological site 8BR04221 within the current APE the artifact assemblage is unexceptional with a lack of stone tools, diagnostic artifacts, and subsurface features, despite the artifact density. However, it is our opinion that archeological sites should be evaluated for their NRHP eligibility as a whole, not in parts. Since 8BR04221 has not been fully delineated and the remainder (outside of the APE) has not been sufficiently evaluated, the site could still be eligible.

- Furthermore, as site 8BR04221 has not yet been sufficiently investigated, we recommend avoidance of 8BR04221 by all proposed project activities.

Otherwise, we have no objections or other comments currently. Please notify our office if any archaeological, historical, and/or burial resources are inadvertently discovered during project implementation and feel free to contact us with any questions or concerns.

Respectfully,

Micheline Hilpert, MA
Compliance Analyst I
STOF THPO, Compliance Section
30290 Josie Billie Hwy, PMB 1004
Clewiston, FL 33440
Email: MichelineHilpert@semtribe.com

From: Owen, Catherine <Catherine.Owen@dot.state.fl.us>
Sent: Tuesday, April 9, 2024 2:46 PM
To: THPO Compliance <THPOCompliance@semtribe.com>
Cc: Rothrock, Lindsay <Lindsay.Rothrock@dot.state.fl.us>
Subject: FM# 437983-1 SR 524 from North Friday Road to Industry Road, Brevard County - PD&E Study CRAS

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Mr. Mueller:

Attached please find a transmittal letter and Cultural Resource Assessment Survey (CRAS) conducted in support of a Project Development and Environment (PD&E) Study for improvements to SR 524 in Brevard County. This study had been put on hold pending funding, thus the CRAS (roadway and ponds combined) is dated 2020.

As a result of the archaeological field survey of the project right-of-way and pond footprints, one newly recorded precontact archaeological site, Cocoa Hill (8BR04221), was identified. This site was identified along a proposed access road to proposed Ponds 2A and 2B; the narrow footprint limited delineation of the site to a single transect of shovel tests. As testing outside of the right-of-way is beyond the scope of this project, insufficient information is available to evaluate 8BR04221 for listing in the National Register of Historic Places (NRHP). Although the density of precontact cultural materials was moderate to high, the nature of the artifact assemblage is unexceptional. Considering the limited artifact assemblage and the lack of diagnostic artifacts and subsurface features, it does not appear that the Cocoa Hill site (8BR04221), as expressed within the current project limits, has the potential to yield further information important in the precontact history of the region. In the opinion of SEARCH, insufficient information is available to evaluate 8BR04221 in its entirety; however, the portion of 8BR04221 located within the SR 524 Improvements project limits is recommended ineligible for listing in the NRHP. Due to the limits of delineation, the density of recovered artifacts, and the potential for the site to extend beyond the limits of the archaeological APE, it was recommended that if additional work is

proposed outside of the current right-of-way and pond footprints within 328 feet (100 meters) of 8BR04221, additional archaeological survey will be required. No other archaeological sites or occurrences were identified as part of the cultural resource survey.

The architectural survey resulted in the identification and evaluation of six historic resources within the SR 524 Improvements APE, including one previously recorded resource and five newly recorded resources. SEARCH recommends that the six historic resources (8BR03331, 8BR04195-8BR4198, and 8BR04214) are ineligible for the NRHP due to a lack of significant historic associations and architectural distinction.

Based on the results of this study, it is the opinion of the District that the proposed undertaking will have no effect on NRHP-listed or -eligible historic properties. No further work is recommended for the SR 524 Improvements project. The State Historic Preservation Officer (SHPO) concurred with the CRAS recommendations on October 2, 2020, noting conditional acceptance based upon not using Pond Alternative Sites 2A/2B due to potential cultural impacts. **Note: it was recently confirmed (March 2024) that the final pond analysis resulted in neither Pond Alternative Site 2A nor 2B being selected as preferred pond sites.**

We are respectfully seeking your review and opinion regarding the findings and recommendations presented in the enclosed report and look forward to continuing consultation regarding this project.

Kind regards,

Catherine B. Owen, M.S.
Environmental Specialist IV
District Cultural Resources Coordinator
FDOT District Five
719 S. Woodland Blvd.
DeLand FL 32720
phone (386) 943-5383



Section 4(f) Resources

Florida Department of Transportation

SR 524 FROM FRIDAY ROAD TO INDUSTRY ROAD

District: FDOT District 5

County: Brevard County

ETDM Number: 14321

Financial Management Number: 437983-1-21-01

Federal-Aid Project Number: D518-034-B

Project Manager: Maria Serrano-Acosta

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. § 327 and a Memorandum of Understanding dated May 26, 2022 and executed by the Federal Highway Administration and FDOT. Submitted pursuant 49 U.S.C. § 303.

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Summary and Approval

Resource Name	Facility Type	Property Classification	Owner/Official with Jurisdiction	Recommended Outcome	OEM SME Action
Junny Rios Martinez Park	County Park	Park/Rec Area	City of Cocoa/Brevard County Parks and Recreation Department	No Use	Determination 11-19-2024
Eastern Florida State College Fred Gay Golf Academy	Golf Training Facility	Park/Rec Area	Eastern Florida State College	No Use	Determination 11-19-2024

Junny Rios Martinez Park

Facility Type: County Park

Property Classification: Park/Rec Area

Address and Coordinates:

Address: 2100 Westminster Dr, Cocoa, FL, 32926, USA

Latitude: 28.39010 Longitude: -80.78160

Description of Property:

Resource Location and Size of Property

The Junny Rios Martinez Park (referred to as the Park) is approximately 6 acres in overall size, separated into two areas on either side of Westminster Drive. The west area is approximately 3.46 acres and the east area is approximately 2.5 acres. The southern boundaries of both the west and east areas about the north right of way (ROW) line of SR 524. See **Figure 1-1** for property information for the west and east areas from Brevard County Property Appraiser's office. The Park was named after the memory of a local youth Junny Rios-Martinez as shown on memorial marker at Park in **Figure 1-2**. The property was sold to the City of Cocoa in 1996. The project location map showing the location of the Park is included in the Project-Level Attachments as well as the deed showing property ownership from the County Clerk's website.



Figure 1-1: Brevard County Property Appraiser Information



Figure 1-2: Memorial Marker at Park

Park Property Access

Vehicle access to both the west and east areas of the Park are from driveway entrances located along either side of Westminster Drive, approximately 150 feet north of the northern ROW line of SR 524. Street level photos along Westminster Drive of the vehicle access points are shown on **Figure 1-3**. There are parking areas on both west and east areas. Concrete walking paths within both west and east areas of the Park connect to the existing sidewalk (approx. 6-foot width) along the north ROW line of SR 524. Street level photos of the sidewalk connection points along SR 524 are shown on **Figure 1-4**.

West Vehicle Entrance

East Vehicle Entrance

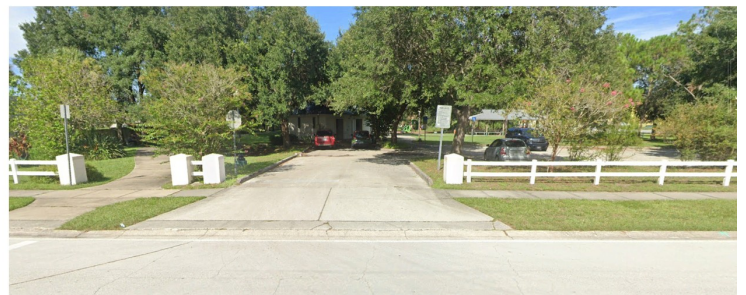


Figure 1-3: Vehicle Entrances to Park

SR 524 Sidewalk Access - West Area



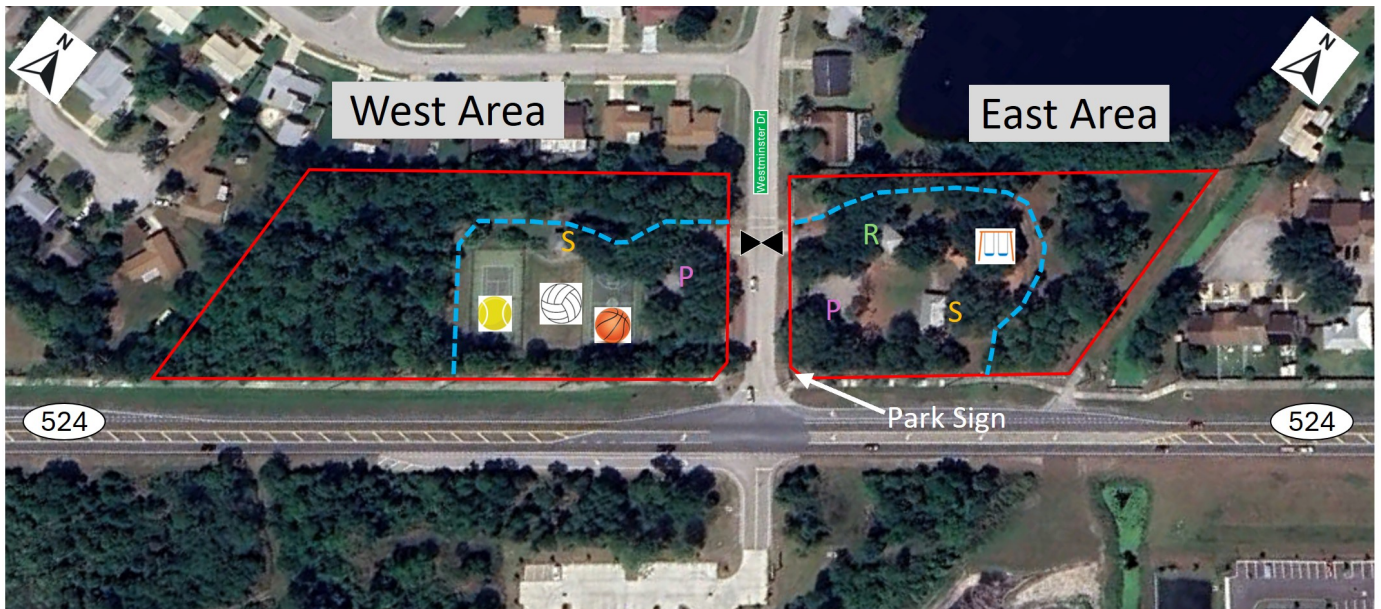
SR 524 Sidewalk Access - East Area



Figure 1-4: Sidewalk Access Points to Park

Park Amenities

The Park provides recreation for local and Brevard County residents. Amenities for the west area include a walking path, tennis court, volleyball court, basketball court, covered shelter/pavilion. Amenities for the east area include a playground, covered shelter/pavilion and restrooms. The west pavilion is 100 square feet with 2 tables. The east pavilion is 1,200 square feet with 8 tables, 2 grills, water and electricity. Patrons rent and reserve use of the pavilions through the Brevard County website. The western portion of the west area is wooded. The eastern portion of the east area is open partially shaded green space. See **Figure 1-5** for location of Park entrances and amenities within the west and east areas. The Park sign located in the northeast corner of the SR 524 intersection with Westminster Drive is shown on **Figure 1-6**.



- Park Area Boundary
- Tennis Court
- Basketball Court
- Volleyball Court
- Restrooms
- Walking Path
- Covered Shelter/Pavilion
- Playground
- Parking Area
- Vehicle Entrances

Figure 1-5: Park Amenities and Access Points Map



Figure 1-6: Park Sign Located at Northwest Corner of SR 524 at Westminster Drive

Owner/Official with Jurisdiction: City of Cocoa/Brevard County Parks and Recreation Department

Relationship Between the Property and the Project

Preferred Alternative Improvements at Park location

See **Figure 1-7** for the proposed SR 524 improvements in relation to the Park property from the Concept Plans. The entrances to the west and east areas of the Park are not located along SR 524 and are located along Westminster Drive. The Park entrances will remain as existing. The SR 524 intersection with Westminster Drive is currently unsignalized and will remain unsignalized with the Preferred Alternative. The Preferred Alternative provides a full median opening at the Westminster Drive location, so full vehicle access to and from SR 524 at Westminster Drive will remain as existing. An eastbound left turn lane and a westbound right turn lane will be provided along SR 524 at the Westminster Drive intersection.

The proposed SR 524 northern roadway edge of pavement will not move closer to the Park than the existing edge of pavement. The existing 6-foot sidewalk along the north ROW line of SR 524 will be replaced with a wider 14-foot-wide shared use path with the same connections to the west and east areas of the Park.

Concept Plan Sheet 9

Concept Plan Sheet 10

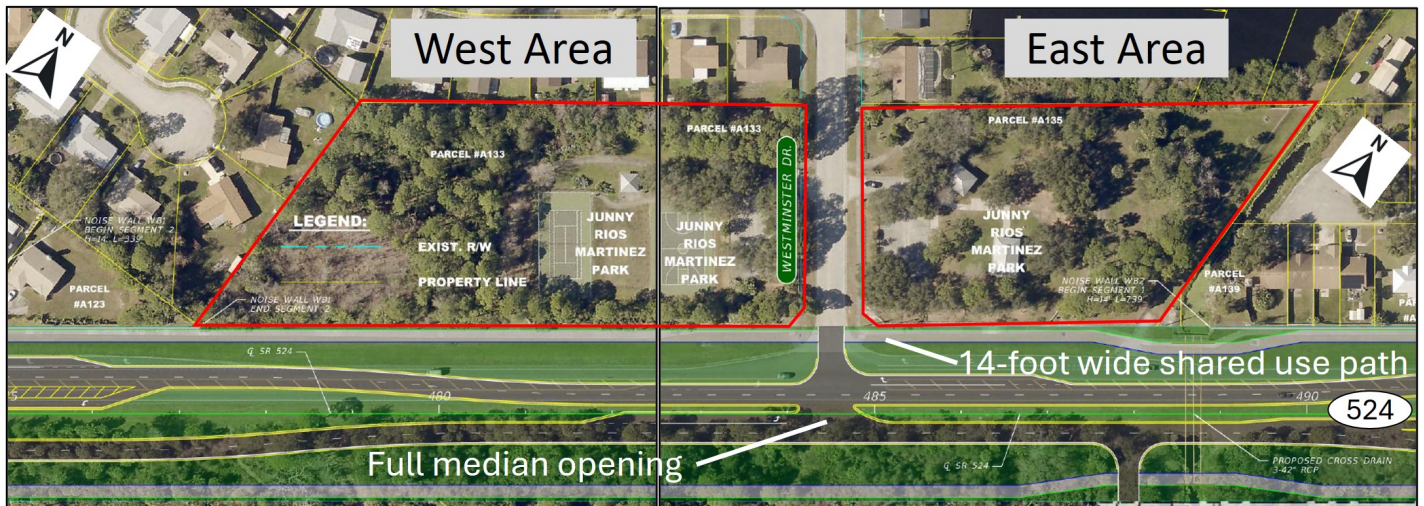


Figure 1-7: Concept Plans Showing Project Improvements Adjacent to Park

Impact Evaluation

The SR 524 improvements will not directly impact any element or amenity within the west or east areas of the Park property, nor obstruct the existing Park sign. There are no off-site stormwater management ponds proposed within or adjacent to the limits of the Park. There will be no temporary occupancy for grading or to provide staging or access areas for the project.

A *Noise Study Report* was prepared for this project. The projected noise level for the west and east areas of the Park does not exceed the relevant threshold in paragraph f(2) of CFR 774.15(f). The existing average noise level for the west and east Park areas is 64.7 A-weighted decibels (dB[A]). With the proposed improvements, the traffic noise for both sites will decrease to an average of 63.6 dB(A). At 63.7 and 63.5 dB(A). Neither west nor east area receptors are predicted to exceed the Noise Abatement Criteria (Category C, which is 67.0 dB[A]) after the project is built. There are no noise impacts predicted for the Park receptors; therefore, abatement consideration is not required.

No acquisition or occupation of land from the protected properties, on either a temporary or permanent basis, will occur. Additionally, there are no meaningful proximity impacts to the protected properties, and there will be no impacts to the access and usage of the protected properties.

Yes No

Will the property be "used" within the meaning of Section 4(f)?

Recommended Outcome: No Use

OEM SME Determination Date: 11-19-2024

Eastern Florida State College Fred Gay Golf Academy

Facility Type: Golf Training Facility

Property Classification: Park/Rec Area

Address and Coordinates:

Address: 3570 State Road 524, Cocoa, FL, 32926, USA

Latitude: 28.39550 Longitude: -80.77419

Description of Property:

Location and Size of Property

The Fred Gay Golf Academy (referred to as the Golf Academy) is approximately 13.47 acres in size. The southern boundary abuts the northerly right of way (ROW) line of SR 524 between Lance Boulevard to the west and London Boulevard to the east. See **Figure 2-1** for property information from Brevard County Property Appraiser's office. The property was sold to the Brevard Community College, now named Eastern Florida State College, in 1976. The golf academy was named for businessman Fred Gay in 2008, who was a longtime supporter of the college's athletic programs. The project location map showing the location of the Golf Academy is in the Project-Level Attachments.



Figure 2-1: Property Information from Brevard County Property Appraiser Website

Park Property Access

Vehicle access to the Golf Academy is from a driveway entrance located along the north side of SR 524 approximately 875 feet east of Lance Boulevard. A street level photo along SR 524 showing the Golf Academy access point is shown on **Figure 2-2**. There is a gate which restricts access outside of operating hours. The Eastern Florida State College website

indicates the hours of operation are seven days a week from 9 a.m. to 5:15 p.m. Pedestrian access utilizes the same driveway entrance as vehicles. A chain link fence separates the Golf Academy facilities from SR 524 and adjacent properties.

Vehicle Entrance along North Side of SR 524



Figure 2-2: Vehicle Access to Golf Academy from SR 524

Golf Academy Amenities

The Golf Academy provides recreation for local residents and Eastern Florida State College students. The facility includes a public tee (100 yards long by 12 yards wide), a 6,500-square-foot putting green, two short-game areas with a practice bunker, a full-size classroom for college students, and a team room for members of the Eastern Florida State College men's and women's golf teams. See **Figure 2-3** for map showing the location of the Golf Academy entrance and amenities. The Golf Academy's sign located along the north side of SR 524 on Golf Academy property is shown on **Figure 2-4**.



Figure 2-3: Golf Academy Entrance and Amenities



Figure 2-4: Golf Academy Sign

Owner/Official with Jurisdiction: Eastern Florida State College

Relationship Between the Property and the Project

Preferred Alternative Improvements in Relation to Golf Academy

See **Figure 2-5** for the proposed SR 524 improvements in relation to the Resource property from the Concept Plans. The unsignalized entrance to the Golf Academy is located along the north side of SR 524. The existing entrance will remain. Vehicle access to and from the Golf Academy with the Preferred Build Alternative is shown on **Figure 2-6**. The ingress from the east and egress to the west will remain as in the existing condition. A directional median opening providing a dedicated left turn lane for vehicles along SR 524 is proposed which will provide ingress access to the Golf Academy from the west. Egress from the Golf Academy to the east will require vehicles to turn right onto SR 524, travel approximately 800-feet to the west and make a u-turn at the proposed median opening at Lance Boulevard to head east on SR 524.

The proposed SR 524 northern roadway edge of pavement will not move closer to the Golf Academy than the existing edge of pavement. The existing sidewalk along the north ROW line of SR 524 will be replaced with a wider 14-foot-wide shared use path. The shared use path will be connected to the Golf Academy entrance at the same location as the existing SR 524 sidewalk connection.

Concept Plan Sheet 11



Concept Plan Sheet 12

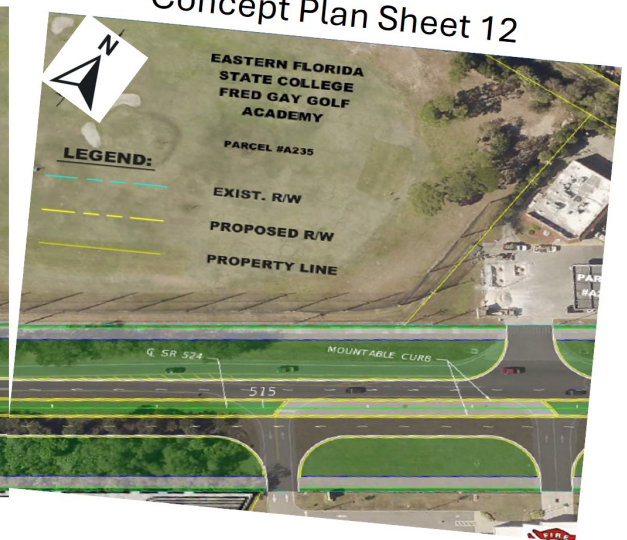


Figure 2-5: Concept Plans Showing Project Improvements Adjacent to Golf Academy

Concept Plan Sheets 10-12

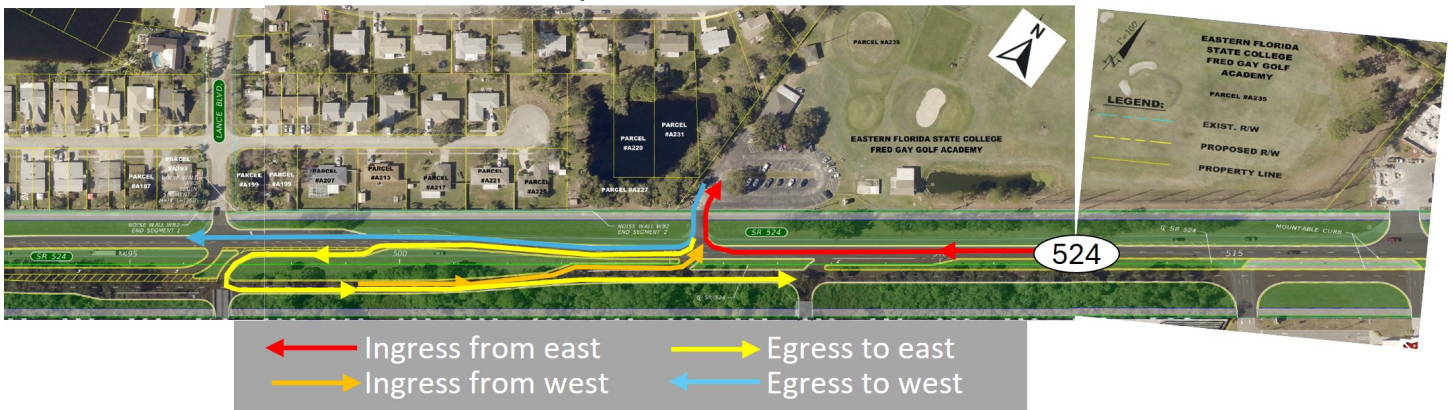


Figure 2-6: Proposed Access to and from the Golf Academy

Impact Evaluation

The SR 524 improvements will not directly impact any element or amenity within the Golf Academy, nor obstruct the existing Golf Academy sign. There are no off-site stormwater management ponds proposed within the limits of the Golf Academy. There will be no temporary occupancy for grading or to provide staging or access areas for the project.

A *Noise Study Report* was prepared for this project. The projected noise level for the Golf Academy does not exceed the relevant threshold in paragraph f(2) of CFR 774.15(f). The existing and no-build noise levels for the Golf Academy are 59.8 and 60.6 dB[A] respectively . With the proposed improvements, the predicted noise level is 58.3 dB(A), a 1.5 dB(A) decrease from existing condition. There are no noise impacts predicted for the Golf Academy receptor; therefore, abatement consideration is not required.

No acquisition or occupation of land from the protected properties, on either a temporary or permanent basis, will occur. Additionally, there are no meaningful proximity impacts to the protected properties, and there will be no impacts to the access and usage of the protected properties. Therefore, the project will have No Use of this recreational Section 4(f) resource.

Yes No

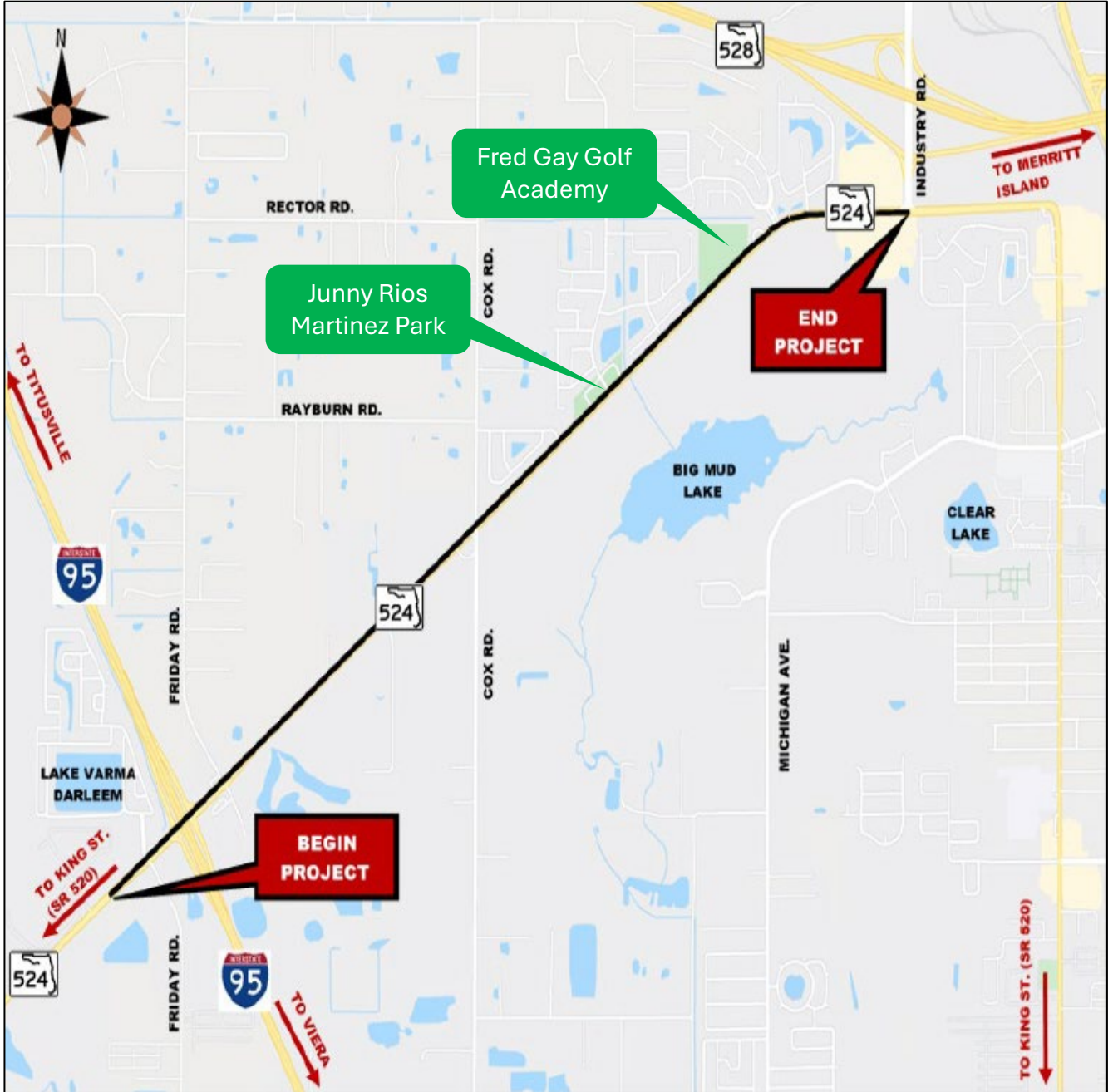
Will the property be "used" within the meaning of Section 4(f)?

Recommended Outcome: No Use

OEM SME Determination Date: 11-19-2024

Project-Level Attachments

Location Map with Section 4(f) Resources



SR 524 PD&E Study
From N. Friday Road to Industry Road
FPID No. 437983-1-22-01
Brevard County

Project Location Map Showing Section 4(f) Resources

Resource Attachments

Junny Rios Martinez Park

Location Map Junny Rios Martinez Park

Junny Rios Martinez Park - Ownership Deed to City of Cocoa

Eastern Florida State College Fred Gay Golf Academy

Location Map Fred Gay Golf Academy

Junny Rios Martinez Park

Contents:

Location Map Junny Rios Martinez Park

Junny Rios Martinez Park - Ownership Deed to City of Cocoa

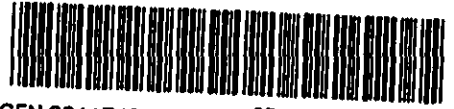


SR 524 PD&E Study
From N. Friday Road to Industry Road
FPID No. 437983-1-22-01
Brevard County

Project Location Map Junny Rios Martinez Park

This Document Prepared By:

MICHAEL M. M. WALLIS, ESQUIRE!
MOSLEY, WALLIS & WHITEHEAD, P.A.
1221 E. NEW HAVEN AVENUE
MILBOURNE, FL. 32901



CFN 96117493 07-08-98 02:05 pm
OR Book/Page. 3586/ 4043

44524

Parcel ID Number: 24-35-13-00-525/24-35-13-00-521
Grantee #1 ITN:

Warranty Deed

This Indenture, Made this 28th day of June, 1996 A.D., Between FIRST FEDERAL SAVINGS AND LOAN ASSOCIATION OF OSCEOLA COUNTY, a corporation existing under the laws of the United States of America

of the County of OSCEOLA, State of Florida, grantor, and CITY OF COCOA, FLORIDA, A FLORIDA MUNICIPAL CORPORATION,

whose address is: 603 BREVARD AVENUE, COCOA, Florida 32926

of the County of BREVARD, State of Florida, grantee.

Witnesseth that the GRANTOR, for and in consideration of the sum of TEN & NO/100(\$10.00) DOLLARS, and other good and valuable consideration to GRANTOR in hand paid by GRANTEE, the receipt whereof is hereby acknowledged, has granted, bargained and sold to the said GRANTEE and GRANTEE'S successors and assigns forever, the following described land, situate, lying and being in the County of BREVARD State of Florida to wit:

SEE EXHIBIT "A" ATTACHED HERETO AND MADE A PART HEREOF.

Subject to: comprehensive land use plans, zoning, restrictions, prohibitions and other requirements imposed by governmental authority; restrictions and matters appearing on the plat or otherwise common to the subdivision; public utility easements of record; taxes for the year 1996 and subsequent years.

Sandy Crawford

Clerk Of Courts, Brevard County

#Pgs 1 #Names 2
Trust 1 00 Rec 5 00 Serv 0 00
Deed 735 00 Excise 0 00
Mtg 0 00 Int Tax 0 00

and the grantor does hereby fully warrant the title to said land, and will defend the same against lawful claims of all persons whomsoever.

In Witness Whereof, the grantor has hereunto set his hand and seal the day and year first above written. Signed, sealed and delivered in our presence:

FIRST FEDERAL SAVINGS AND LOAN ASSOCIATION OF OSCEOLA COUNTY

Printed Name: MICHAEL M. WALLIS

Witness: DORIS MCGONAGLE

Printed Name: DORIS MCGONAGLE

Witness

By: JAMES W. BURNS (Seal)
JAMES W. BURNS, VICE-PRESIDENT
P.O. Address 200 E. BROADWAY, KISSIMMEE, FL 34741

(Corporate Seal)

STATE OF FLORIDA
COUNTY OF BREVARD

The foregoing instrument was acknowledged before me this 28 day of June, 1996 by JAMES W. BURNS, VICE-PRESIDENT of FIRST FEDERAL SAVINGS AND LOAN ASSOCIATION OF OSCEOLA COUNTY, (the United States of America Corporation), on behalf of the corporation. He is personally known to me.

Printed Name: MICHAEL M. WALLIS
NOTARY PUBLIC My Commission Expires 11/27/99



MICHAEL M. WALLIS
My Commission Expires 11/27/99
Notary Public
800-422-1885

LEGAL DESCRIPTION

Parcel A

Beginning at the Southeast corner of Cocoa North 7, as recorded in P.B. 30, Pages 33 & 34, of the Public Records of the City of Cocoa, Brevard County, Florida. Thence S.53°01'12"W 529.80 feet to the Northeasterly R.O.W. of Westminster Drive; thence S36°58'48"E along said R.O.W. 225.01 feet to a curve concave to the North; thence along said curve having a radius of 25.00 feet, a central angle of 90°00'00" and a length of 39.27' to the Northwest R.O.W. of S.R. 524; thence N53°01'12"E, along said R.O.W. 318.57 feet to the East line of the Southwest ¼ of Section 13, Township 24 South, Range 35 East, thence N00°17'11"W, along said Section 311.79 feet to the Point of Beginning for this description.

Parcel B

Commence at the South ¼ corner of Section 13, Township 24 South, Range 35 East, Brevard County, Florida; thence run N.00 degrees 18'01"W, along said section 664.40 feet to the North right-of-way line of State Road No. 524; thence run S.53 degrees 01'12"W along said right-of-way 440.00 feet to the POB; thence continue S.53 degrees 01'12"W 670.33 feet; thence run N.00 degrees 08'28"W., 312.38 feet; thence run N.53 degrees 01'12"E., 508.04 feet; thence run S.36 degrees 58'48"E., 225.00 feet; thence run along the arc of a curve concave Southwesterly, having it's elements a radius of 25.00 feet, a central angle of 90 degrees 00'00" and an arc distance of 39.27 feet to the POINT OF BEGINNING. Said parcel containing 3.4502 acres, more or less.

Eastern Florida State College Fred Gay Golf Academy

Contents:

Location Map Fred Gay Golf Academy



SR 524 PD&E Study
From N. Friday Road to Industry Road
FPID No. 437983-1-22-01
Brevard County

Project Location Map Fred Gay Golf Academy

Natural Resources Appendix

Contents:

USFWS Concurrence on Scrub-jay and Caracara Survey (December 2019)

USFWS Concurrence on Eastern Black Rail Evaluation (December 2024)

FWC Letter on NRE Effect Determinations (April 2025)



Florida Department of Transportation

RON DESANTIS
GOVERNOR

719 S. W
DeLand,

KEVIN I THIRALL, P.E.

November 13, 2019

Zakia Williams
U.S. Fish & Wildlife Service
North Florida Ecological Services Office
7615 Baymeadows Way, Suite 200
Jacksonville, FL 32256-7517



FWS Log No 2020-TA-0218

The Service concurs with your effect determination(s) for resources protected by the Endangered Species Act of 1973, as amended (16 U.S.C. 1531 et seq.). This finding fulfills the requirements of the Act.

Jay B. Herrington
Field Supervisor

12/10/19
Date

Re: Preliminary Florida Scrub-Jay & Caracara Survey and Evaluation
SR 524 Widening from Friday Rd to Industry Rd
Brevard County, Florida
FDOT FM 437983-1-22-01

Ms. Williams:

The Florida Department of Transportation (FDOT) District 5 is conducting a Project Development and Environmental Study (PD&E) to widen SR 524 from two lanes to four lanes in the City of Cocoa, Florida (Sections 13, 23 and 24; Township 24 South; Range 35 East and Section 18; Township 24 South; Range 36 East). The project length is approximately 3.22 miles, extending from west of Friday Road to Industry Road and includes nine (9) potential pond site alternatives (**Figures 1 - 5**).

The project is located within the United States Fish & Wildlife Service (USFWS) consultation areas (CA) for the Florida scrub-jay (*Aphelocoma coerulescens*) and the Audubon's crested caracara (*Polyborus plancus audubonii*). As a result, FDOT consultants conducted a desktop Geographic Information System (GIS) review and subsequent onsite surveys to assess the available habitat within the project right-of-way and 9 potential pond site alternatives.

Methods

Prior to conducting an onsite habitat assessment, the following GIS data was used to evaluate potential habitat for the Florida scrub-jay and the Audubon's crested caracara.

- USFWS Consultation Area GIS data layers;
- Florida Fish & Wildlife Conservation Commission (FWC) Wildlife Observations: FWC Wildlife Occurrence System, (2016) data;

www.fdot.gov

- FWC Fish and Wildlife Research Institute Florida Scrub-jay Locations (1992-1993, updated 2015) data
- Brevard County's Natural Resources Florida Scrub-jay GIS data
- St. Johns River Water Management District 2014 Florida Land Use, Cover and Forms Classification System (FLUCCS) data.

Following the GIS desktop review, an onsite survey of the project corridor including the 9 potential pond site alternatives (**Figure 5**) was conducted on April 12 and April 25, 2019. The onsite surveys were specifically focused on identifying potential Audubon's crested caracara foraging or nesting habitat (improved/unimproved pastures and/or wet/dry prairies with cabbage palms) and Florida scrub-jay habitat (Types I, II and III) The onsite surveys used pedestrian transects and a playback of high quality recordings of typical Florida scrub-jay territorial scold calls in an attempt to attract scrub-jays.

Site Descriptions and Results

The project right-of-way consists mainly of periodically mowed; open land adjacent to multiple land uses (**Figure 4**). The areas of ROW adjacent to potential scrub-jay and caracara habitat were field reviewed and a tape of scrub-jay vocalizations was played. No scrub-jays, caracara or suitable habitat were observed.

Pond 1 is mapped as Hardwood Coniferous Mixed (FLUCCS 434). There was evidence of inundation due to water staining on trees. The site contains dense vegetation consisting of Brazilian pepper (*Schinus terebinthifolia*), punktree (*Melaleuca quinquenervia*), cabbage palm (*Sabal palmetto*), salt bush (*Baccharis halimifolia*), laurel oak (*Quercus laurifolia*), red maple (*Acer rubrum*), saw palmetto (*Serenoa repens*), gallberry (*Ilex glabra*), wax myrtle (*Morella cerifera*), and leather fern (*Acrostichum danaeifolium*). No appropriate Florida scrub-jay or Audubon's crested caracara habitat occurs within this potential pond site and neither of these species were observed during the onsite surveys. Representative photos of this pond site are identified as Photos 1 – 4 in the attached photolog.

Pond 2 is mapped as Pine Flatwoods (FLUCCS 411). During the site review the site was actively being cleared and likely recently burned. The remaining vegetation included slash pine (*Pinus elliottii*), cabbage palm and punktree in the adjacent off-site area. No appropriate Florida scrub-jay or Audubon's crested caracara habitat occurs within this potential pond site and neither of these species were observed during the onsite surveys. Representative photos of this pond site are identified as Photos 5 – 6 in the attached photolog.

Pond 3 is mapped as Pine Flatwoods (FLUCCS 411). The northern half of the site appears to be a wetland area containing gallberry, shiny lyonia (*Lyonia lucida*), Virginia chain fern (*Woodwardia virginica*), sphagnum moss (*Sphagnum* sp.), wiregrass (*Aristida stricta*), and red maple with a canopy of slash pine. Saw palmetto (*Serenoa repens*) was observed throughout the pond site with other vegetation including St. John's Wort (*Hypericum* sp.), wax myrtle, punktree, pond pine (*Pinus serotina*) and rusty lyonia (*Lyonia ferruginea*). Evidence of seasonal inundation was observed on some of the wetter portions of the pond

site. No appropriate Florida scrub-jay or Audubon's crested caracara habitat occurs within this potential pond site and neither of these species were observed during the onsite surveys. Representative photos of this pond site are identified as Photos 7 – 10 in the attached photolog.

Pond 4 is mapped as Herbaceous (Dry Prairie) (FLUCCS 310). The site is maintained with periodic mowing with scattered slash pine and contains an excavated surface water in the northwest corner. The pond is vegetated with white waterlily (*Nymphaea odorata*) and the banks are vegetated with cabbage palm, dog fennel (*Eupatorium capillifolium*), slash pine, and Virginia chain fern. The western border of the site contains wax myrtle, button bush (*Cephalanthus occidentalis*), red maple, and St. John's wort. No appropriate Florida scrub-jay or Audubon's crested caracara habitat occurs within this potential pond site and neither of these species were observed during the onsite surveys. Representative photos of this pond site are identified as Photos 11 – 14 in the attached photolog.

Pond 5 is mapped as shrub and brushland (FLUCCS 320). The site is dominated by a canopy of pines near SR 524 and they occur more sporadically at the south end of the site. There was evidence of clearing activities along the perimeter and center of the site. Dominant vegetation included gallberry, saw palmetto, wiregrass, muscadine grapevine (*Vitis rotundifolia*), shiny blueberry (*Vaccinium myrsinites*), cabbage palm, and live oak. The eastern portion of the site is mapped as having potential scrub-jay habitat by the FWC. However, a review of Brevard County's Natural Resources GIS data shows no known Florida scrub-jay areas near this pond site. In addition, no appropriate Florida scrub-jay habitat was observed within this potential pond site and this species was not observed during the onsite surveys. Additionally, no appropriate Audubon's crested caracara habitat occurs within this potential pond site and this species was not observed during the onsite surveys. Representative photos of this pond site are identified as Photos 15 – 18 in the attached photolog.

Pond 6 is mapped as commercial and services (FLUCCS 140). This pond site is currently undeveloped open land that is maintained with periodic mowing. The habitat consists exclusively of uplands and herbaceous vegetation is dominated by pasture grasses including bahiagrass (*Paspalum notatum*), Bermudagrass (*Cynodon dactylon*), ragweed (*Ambrosia artemisiifolia*) and Mexican clover (*Richardia brasiliensis*). There are no trees or shrubs within the limits of this potential pond site. No appropriate Florida scrub-jay or Audubon's crested caracara habitat occurs within this potential pond site and neither of these species were observed during the onsite surveys. Representative photos of this pond site are identified as Photos 19 – 20 in the attached photolog.

Pond 7 is mapped as commercial and services (FLUCCS 140). This pond site is currently undeveloped open land that is maintained with periodic mowing. The habitat consists exclusively of uplands and herbaceous vegetation that is dominated by pasture grasses including bahiagrass, Bermudagrass, ragweed and Mexican clover. There are no trees or shrubs within the limits of this potential pond site. No appropriate Florida scrub-jay or Audubon's crested caracara habitat occurs within this potential pond site and neither of these species were observed during the onsite surveys. Representative photos of this pond site are identified as Photos 21 – 22 in the attached photolog.

Pond 8 is mapped as Herbaceous (Dry Prairie) (FLUCCS 310) and Pine Flatwoods (FLUCCS 411). Most of this site is dominated by saw palmetto and rusty lyonia with live oak and scattered sand live oak (*Quercus geminata*). Other vegetation observed included muscadine grapevine, earleaf greenbrier (*Smilax auriculata*), pawpaw (*Asimina* sp.) and American beautyberry (*Callicarpa americana*). No canopy exists over most of this site and evidence of the historic pine canopy (circa 2010) is limited to scattered snags. Because of the lack of fire maintenance, the shrub component is very dense with minimal herbaceous ground cover species and no observed areas of open sand. This site is mapped as having potential scrub-jay habitat by the FWC and plant species typical of scrub habitat were observed. However, a review of Brevard County's Natural Resources GIS data shows no known Florida scrub-jay areas near this pond site. In addition, the lack of fire management and subsequent density of the shrub layer resulted in no appropriate Florida scrub-jay habitat being observed within this potential pond site and this species was not observed during the onsite surveys. Additionally, no appropriate Audubon's crested caracara habitat occurs within this potential pond site and this species was not observed during the onsite surveys. Representative photos of this pond site are identified as Photos 23 – 26 in the attached photolog.

Pond 9 is mapped as Mixed Wetland Hardwoods (FLUCCS 617) and Vegetated Non-forested wetland (FLUCCS 640). The only available uplands consist of the northern and eastern fringes of the ponds site that are vegetated with live oak, water oak (*Quercus nigra*) and slash pine. The majority of the site consists of wetlands with the central portion consisting of an inundated stand of Carolina willow (*Salix caroliniana*). Other vegetation found throughout this pond site includes red maple, sweetgum (*Liquidambar styraciflua*) Virginia chain fern, leather fern, swamp fern (*Telmatoblechnum serrulatum*), royal fern (*Osmunda regalis*), muscadine grapevine, and lizard's tail (*Saururus cernuus*). No appropriate Florida scrub-jay or Audubon's crested caracara habitat occurs within this potential pond site and neither of these species were observed during the onsite surveys. Representative photos of this pond site are identified as Photos 27 – 30 in the attached photolog.

Conclusions

The FDOT respectfully requests your concurrence with the conclusions of our findings and that no other scrub-jay or Audubon's crested caracara survey effort will be required for this road widening project. If you have any questions or need any additional information, please do not hesitate to contact me at heather.chasez@dot.state.fl.us or via phone at (386) 943-5393

Sincerely,

Heather Chasez
Environmental Specialist IV
Project Compliance Coordinator

Attachments:

Figure 1. Project Overview Map

Figure 2. USGS Topographic Map

Figure 3. Soils Map

Figure 4. Land Use Map

Figure 5. Scrub-Jay Habitat & Playback Station Map

Photolog



Florida Department of Transportation

RON DESANTIS
GOVERNOR

719 S. W
DeLand,



Florida Ecological Services Field Office

Service Project
Code No. 25-I-0021408

The U.S. Fish and Wildlife Service has reviewed the information provided and finds that the proposed action is not likely to adversely affect any federally listed species or designated critical habitat protected by the Endangered Species Act of 1973 (Act), as amended (16 U.S.C. 1531 et. seq.). A record of this consultation is on file at the Florida Ecological Services Field Office.

This fulfills the requirements of section 7 of the Act and further action is not required. If modifications are made to the project, if additional information involving potential effects to listed species becomes available, or if a new species is listed, reinitiation of consultation may be necessary.

CATRINA MARTIN

Digitally signed by CATRINA
MARTIN

Date: 2024.12.02 14:47:47 -06'00'

Supervisor

October 25, 2024

Zakia Williams
U.S. Fish & Wildlife Service
North Florida Ecological Services Office
7615 Baymeadows Way, Suite 200
Jacksonville, FL 32256-7517

Re: Preliminary Eastern Black Rail Habitat Suitability Evaluation
SR 524 Widening from Friday Road to Industry Road
Brevard County, Florida
FDOT FM 437983-1-22-01

Ms. Williams:

The Florida Department of Transportation (FDOT) District 5 is conducting a Project Development and Environmental Study (PD&E) to widen SR 524 from two lanes to four lanes in the City of Cocoa, Florida (Sections 13, 23, and 24; Township 24 South; Range 35 East and Section 18; Township 24 South; Range 36 East). The project length is approximately 3.22 miles, extending from west of Friday Road to Industry Road and includes four preferred pond sites (**Figures 1 – 5**).

The project is located within the United States Fish & Wildlife Service (USFWS) consultation area (CA) for the Eastern black rail (*Laterallus jamaicensis*). As a result, FDOT consultants conducted a desktop Geographic Information System (GIS) review and subsequent onsite survey to assess the suitability of potential habitat within the four preferred pond sites and the project right of way.

Methods

Prior to conducting an onsite habitat assessment, the following GIS data was used to evaluate potential habitat for the Eastern black rail.

- USFWS Consultation Area GIS data layer;
- St. Johns River Water Management District 2020 Florida Land Use, Cover and Forms Classification System (FLUCCS) data;
- United States Geological Survey 1-Meter Digital Elevation Model.

www.fdot.gov

Ms. Williams, USFWS
FDOT FM # 437983-1-22-01
Page 2 of 5

Following the GIS desktop review, an onsite survey of the project corridor including the four preferred pond site alternatives (**Figure 1**) was conducted on September 19, 2024. A scientist with previous Eastern black rail survey experience led the survey (see **Surveyor Resume**). The onsite survey was specifically focused on identifying and assessing potential Eastern black rail foraging or nesting habitats, which includes a variety of salt, brackish, and freshwater marsh habitats as well as wet prairie habitats. The two major factors for consideration in suitable Eastern black rail habitat are vegetation structure and hydrology. According to the USFWS, plant structure is considered more important than plant species composition in predicting habitat suitability. Ideal vegetation height is around three to four feet above land surface, but if shrub densities are too high, the habitat becomes less suitable. The soils of suitable habitat are moist to saturated, occasionally dry, and adjacent to very shallow water of one to two inches.

Site Descriptions and Results

The project right of way consists mainly of periodically mowed; open land (see **Photolog**) adjacent to multiple land uses (**Figures 4A-D**). The topography of the project area and the soil types within the project area are described in **Figure 2** and **Figures 3A-3D**. The areas of right of way adjacent to potential Eastern black rail habitat were field reviewed for suitability. Of the twelve wetlands identified, only wetlands 1, 3, 10 and 11 contain potential Eastern black rail habitat. No suitable habitat for the Eastern black rail was observed within the footprint of proposed impacts or within approximately 25 feet of proposed impacts.

Pond 1A is located within Wetland 1, that is mapped as a Freshwater Marsh (FLUCCS 641) community; however, the system appears to be transitioning to a mixed scrub-shrub wetland based on the current vegetative structure. Proposed impacts to Wetland 1 include 3.87 acres of impact (**Figure 5A**). Trees within the wetland consist of scattered red maple (*Acer rubrum*), slash pine (*Pinus elliotii*), and punktree (*Melaleuca quinquenervia*). Punktree is encroaching on the remaining areas dominated solely by herbaceous groundcover. Other trees and shrubs along the perimeter include Brazilian pepper (*Schinus terebinthifolia*), Carolina willow (*Salix caroliniana*), cabbage palm (*Sabal palmetto*), dahoon holly (*Ilex cassine*), and wax myrtle (*Morella cerifera*). Herbaceous vegetation is dominated by sawgrass (*Cladium jamaicense*), softrush (*Juncus effusus*), spikerush (*Eleocharis spp.*), and whitetop sedge (*Rhynchospora colorata*). Eastern black rail habitat of very low suitability occurs within this preferred pond site. This species was not observed during the assessment. Representative photos of this preferred pond site are identified as **Photos 1 – 20** in the attached photolog.

The low suitability of Eastern black rail habitat within Pond 1A was assessed based on the following onsite observations. The herbaceous groundcover as the sole vegetative stratum occupies approximately 200 square meters of the 3.5 acres of wetland. Herbaceous groundcover contains grasses, rushes, and sedges but this groundcover does not exhibit the stem density or interspersed vegetation structure preferred by the Eastern black rail. Observations assessing the height of herbaceous vegetation demonstrated that the preferred height of 1 meter is rarely met. At the time of inspection, standing water was approximately 4-5 inches above land surface, higher than the preferred water levels for this species,

Ms. Williams, USFWS
FDOT FM # 437983-1-22-01
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although water levels may decrease to optimal levels during the dry season. The system is bordered by major highways and roads to the south, east, and west. A high berm and adjacent ditches to the north have likely impacted hydrology by reducing the wetland hydroperiod, which could be contributing to the encroachment of invasive exotic species.

Pond 2F is located within Wetland 10, that is mapped as a Mixed Wetland Hardwoods (FLUCCS 617) and Freshwater Marsh (FLUCCS 641). Proposed impacts to Wetland 10 include 1.89 acres of impact within the preferred pond site and 0.17 acres within the right of way (**Figure 5B**). Trees in this community include bald cypress (*Taxodium distichum*), red maple, swamp bay (*Persea palustris*), red bay (*Persea borbonia*), and cabbage palm. Other vegetation included royal fern (*Osmunda regalis*), arrowhead (*Sagittaria lancifolia*), buttonbush (*Cephalanthus occidentalis*), sawgrass, and Virginia chainfern (*Woodwardia virginica*). No appropriate Eastern black rail habitat occurs within or around this preferred pond site nor within approximately 25 feet waterward of the proposed impact to Wetland 10 within the right of way. This species was not observed during the onsite survey. Representative photos of this preferred pond site are identified as **Photos 21 – 27** in the attached photolog.

Pond 3A is mapped as Commercial and Services (FLUCCS 140). This pond site is currently undeveloped open land that is maintained with periodic mowing. The habitat consists exclusively of uplands and herbaceous vegetation dominated by ragweed (*Ambrosia artemisiifolia*), Mexican clover (*Richardia brasiliensis*), and pasture grasses including bahiagrass (*Paspalum notatum*) and Bermudagrass (*Cynodon dactylon*). There are no trees or shrubs within the limits of this preferred pond site. No appropriate Eastern black rail habitat occurs within or around the extent of this preferred pond site and this species was not observed during the onsite survey. Representative photos of this pond site are identified as **Photos 28 – 31** in the attached photolog.

Pond 3B is mapped as Commercial and Services (FLUCCS 140). This pond site is currently undeveloped open land that is maintained with periodic mowing. The habitat consists exclusively of uplands and herbaceous vegetation that is dominated by ragweed, Mexican clover, and pasture grasses including bahiagrass and Bermudagrass. No appropriate Eastern black rail habitat occurs within or around the extent of this preferred pond site and this species was not observed during the onsite survey. Representative photos of this pond site are identified as **Photos 32 – 35** in the attached photolog.

Wetland 3 is located on the north side of the project corridor and abuts the SR 524 right of way. Wetland 3 is part of a larger wet prairie (FLUCCS 643) community that extends offsite. Herbaceous vegetation includes yellow-eyed grass (*Xyris spp.*), beaksedge (*Rhynchospora spp.*), cattail (*Typha spp.*), and bushy bluestem (*Andropogon glomeratus*). Additional vegetation observed in this wetland as it abuts the SR 524 right of way includes Carolina willow, primrose willow (*Ludwigia peruviana*), and salt bush (*Baccharis halimifolia*). While Wetland 3 does include a small emergent wetland, this community type is much further offsite and would not be impacted by this project. The proposed road widening project would only impact 0.27 acres of roadside habitat consisting of mowed vegetation and the immediately adjacent

Ms. Williams, USFWS
FDOT FM # 437983-1-22-01
Page 4 of 5

woody wetland fringe (**Figure 5A**). No appropriate Eastern black rail habitat occurs within this proposed impact area, nor within approximately 25 feet waterward of the proposed impact, where vegetation is dominated by arrowhead. The Eastern black rail was not observed during the onsite survey. Representative photos of this wetland site are identified as **Photos 36 – 37** in the attached photolog.

Wetland 11 is located on the south side of the project corridor and abuts the SR 524 right of way. Wetland 11 is part of a larger wet prairie community that extends offsite and is bisected by a powerline easement. Herbaceous vegetation includes yellow-eyed grass, beaksedge, cattail, and bushy bluestem. Vegetation that covers the fringe of this community as it abuts the SR 524 right of way includes Carolina willow, primrose willow, and salt bush. While Wetland 11 does support a wetland prairie community of approximately 4 acres in its interior, the proposed road widening project would only impact 1.16 acres of roadside habitat consisting of mowed vegetation and the immediately adjacent woody wetland fringe (**Figure 5A**). The impact area does not extend more than approximately 25 feet from the existing paved road surface. No appropriate Eastern black rail habitat occurs within this proposed impact area, nor within approximately 25 feet waterward of the proposed impact, where vegetation is dominated by maidencane (*Panicum hemitomom*) and the water level was over one foot above land surface. The hydrology of this wetland appears to be historically impacted by the construction of multiple roads, ditches, berms and the powerline corridor, likely increasing the hydroperiod of the wetland and decreasing the suitability of potentially available habitat. The Eastern black rail was not observed during the onsite survey. Representative photos of this wetland site are identified as **Photos 38 – 39** in the attached photolog.

Wetlands 2, 5, and 12 did not contain any suitable Eastern black rail habitat adjacent to the right of way due to the presence of trees and shrubs, and lack of emergent marsh vegetation. Representative photos of these sites are identified as **Photos 40 – 46** in the attached photolog.


Conclusions

Based on the above assessment, the proposed SR 524 project is not anticipated to result in adverse effects on the Eastern black rail or their habitat. Eastern black rail habitat was limited to Wetland 1 located near the western extent of the project area within Pond 1A. The suitability of this habitat is low, due to the absence of appropriate vegetation structure, density, and height. No appropriate Eastern black rail habitat was observed in any other wetlands within the project limits, nor within approximately 25 feet waterward of the proposed wetland impacts. Therefore, a “May Affect, Not Likely to Adversely Affect” determination has been made for the Eastern black rail for this project and no additional surveys will be required for this species during the design phase of the project.

The FDOT respectfully requests your concurrence with the conclusions of our findings and that no other Eastern black rail survey effort will be required for this road widening project. If you have any questions or need any additional information, please do not hesitate to contact me at casey.lyon@dot.state.fl.us or via phone at (386) 943-5436.

Ms. Williams, USFWS
FDOT FM # 437983-1-22-01
Page 5 of 5

Sincerely,

DocuSigned by:

389B80E4828F45B...
Casey Lyon, M.S.

Environmental Manager
Florida Department of Transportation – District 5

Attachments:

- Figure 1. Project Overview Map
- Figure 2. USGS Topographic Map
- Figures 3A-3D. Soils Map
- Figures 4A-4D. Land Use Map
- Figures 5A-C. Eastern black rail Habitat Assessment Map
- Photolog
- Surveyor Resume



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*Managing fish and wildlife
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MyFWC.com

April 24, 2025

Edward Northey
Florida Department of Transportation District 5
719 S Woodland Boulevard
Deland, FL 32720
edward.northey@dot.state.fl.us

Re: SR 524 Natural Resource Evaluation (Updated March 2025), Brevard County

Dear Mr. Northey:

Florida Fish and Wildlife Conservation Commission (FWC) staff reviewed the above-referenced Natural Resources Evaluation (NRE) report in accordance with FWC's authorities under Chapter 379, Florida Statutes, and Chapter 68A-27, Florida Administrative Code.

The Florida Department of Transportation (FDOT) District 5 is conducting a Project Development and Environment (PD&E) study (Efficient Transportation Decision Making Project # 14321) on an approximately 3.15-mile segment of State Road (SR) 524 from west of Friday Road to Industry Road in Brevard County. The PD&E study is analyzing design alternatives that widen SR 524 from two lanes to four lanes to provide additional capacity, add bicycle and pedestrian facilities, and address transit needs. New right-of-way (ROW) may be required for offsite ponds as part of the stormwater management facilities. The roadway construction itself will occur within the existing FDOT ROW.

The NRE report was prepared as part of the PD&E study to document wetlands, surface waters, protected species, critical habitat, and essential fish habitat within the project's corridor; evaluate potential impacts associated with the proposed project; provide effect determinations for protected species; identify mitigation needs, and coordinate with federal and state regulatory and resource agencies. FWC staff reviewed and commented on the original NRE dated September 2023. FWC staff agrees with the effect determinations and supports the project implementation measures and commitments for protected species included in the updated NRE. Further coordination could be required during future species-specific surveys and project permitting.

For specific technical questions regarding the content of this letter, please contact Kristee Booth at (850) 363-6298 or Kristee.Booth@MyFWC.com. All other inquiries may be directed to ConservationPlanningServices@MyFWC.com.

Sincerely,

Laura DiGruttolo
Land Use Planning Supervisor
Office of Conservation Planning Services

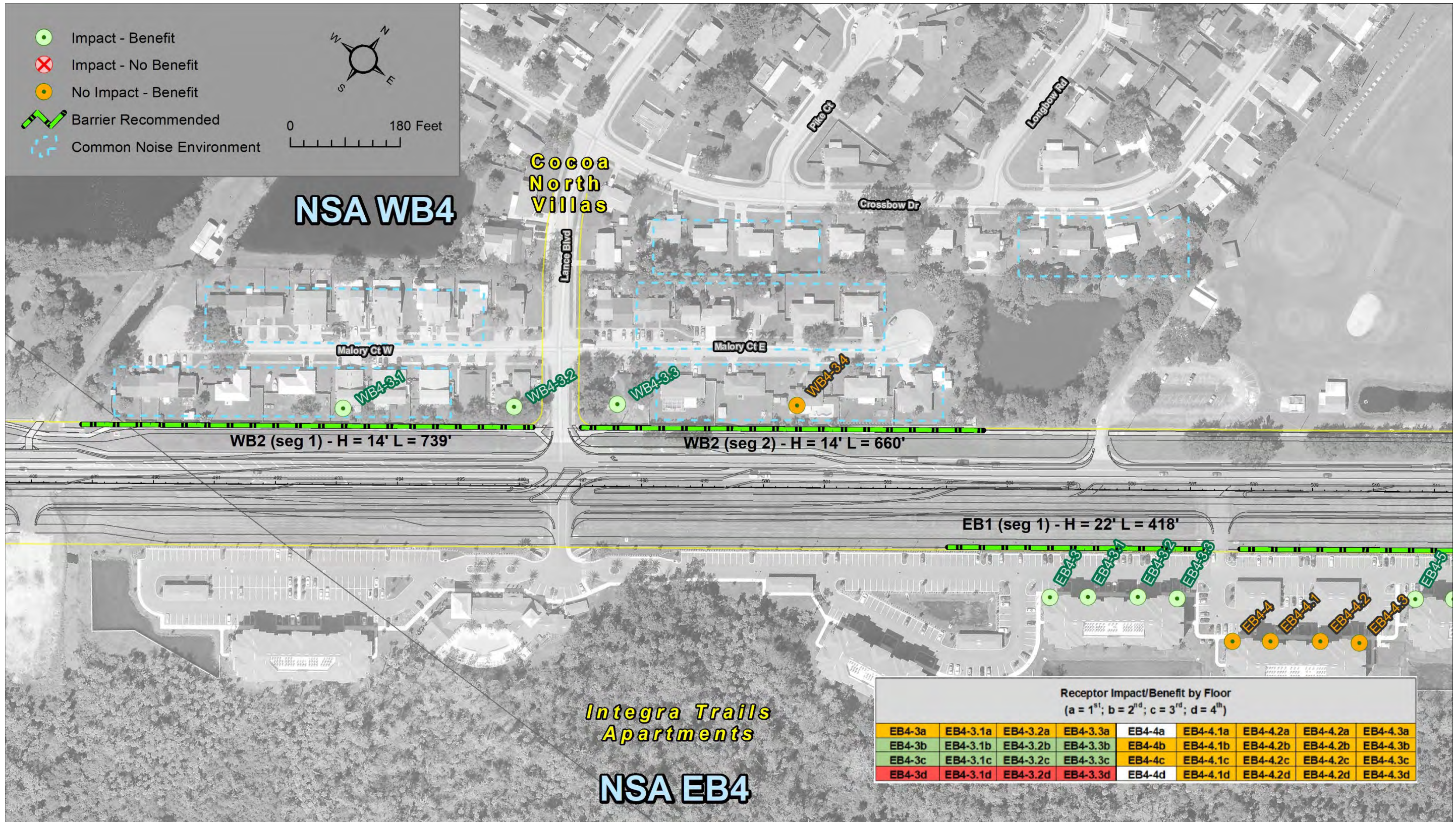
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SR 524 Updated NRE_61531_04242025

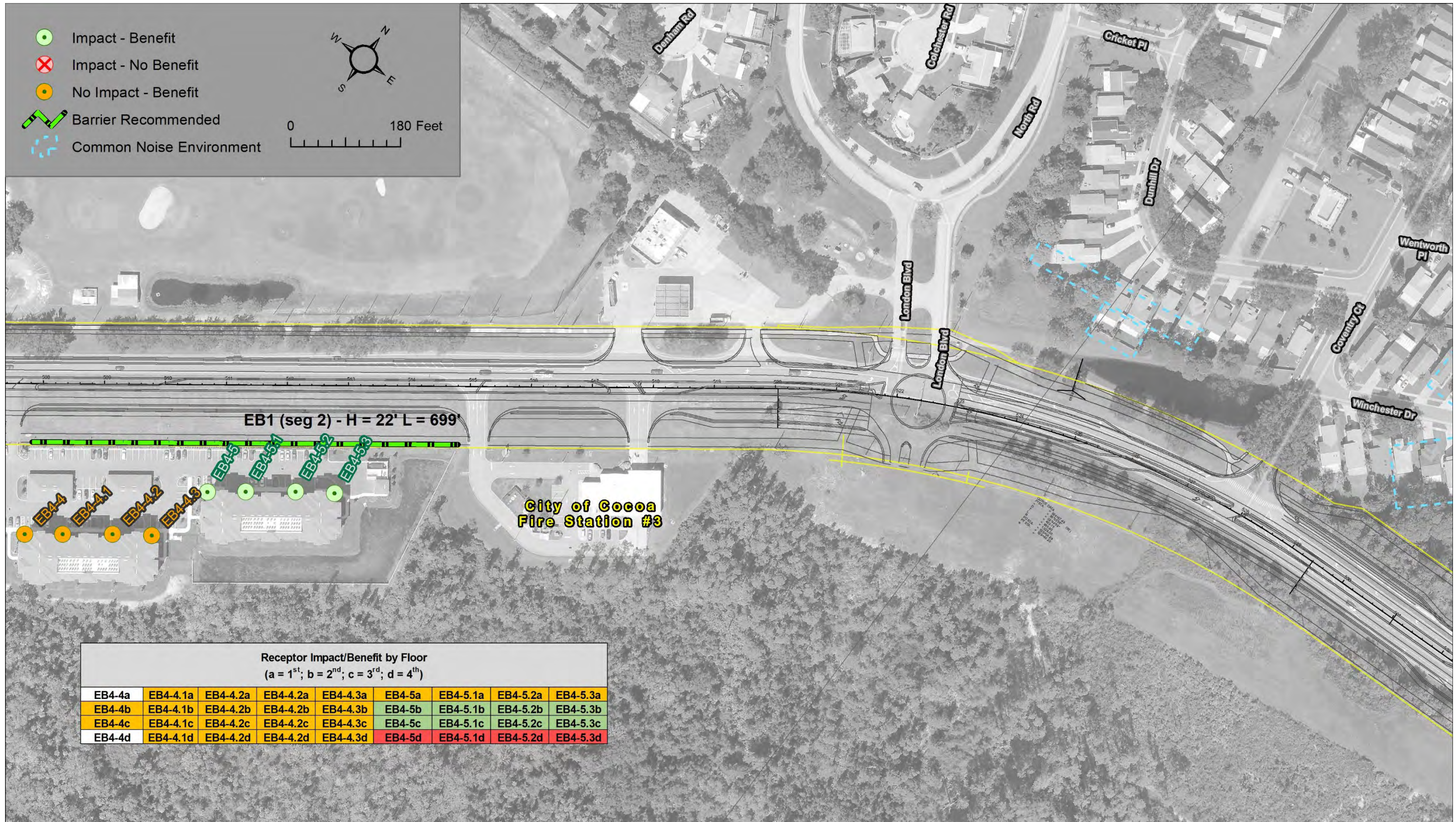
Physical Resources Appendix

Contents:

Noise Barriers Map







Public Involvement Appendix

Contents:

Public Hearing Certification-Virtual Session Jan 20-2026

Public Hearing Certification-In-person Session Jan 22-2026

Public Hearing Transcript - Virtual Session (February 2026)

Public Hearing Transcript - In-Person Session (February 2026)

PUBLIC HEARING CERTIFICATION

SR 524 FROM FRIDAY ROAD TO INDUSTRY ROAD

Project Development and Environment (PD&E) Study

from From Friday Road to Industry Road

Brevard County, Florida

Financial Management No.: 437983-1-21-01

I certify that a public hearing was conducted on 01/20/2026, beginning at 05:30 PM for the above project. A transcript was made and the document attached is a full, true, and complete transcript of what was said at the hearing.

Maria Serrano-Acosta

(Name)

February 26, 2026

Date

Project Manager

(Title of FDOT Representative)



Link to Public Hearing Transcript

- 1 - [43798312101-CE2-D5-Public_Hearing_Transcript_of_Virtual_Session_-2026-0206.pdf](#)

PUBLIC HEARING CERTIFICATION

SR 524 FROM FRIDAY ROAD TO INDUSTRY ROAD

Project Development and Environment (PD&E) Study

from From Friday Road to Industry Road

Brevard County, Florida

Financial Management No.: 437983-1-21-01

I certify that a public hearing was conducted on 01/22/2026, beginning at 05:30 PM for the above project. A transcript was made and the document attached is a full, true, and complete transcript of what was said at the hearing.

Mark Trebitz

(Name)

February 27, 2026

Date

SWEPT FDOT Project Development Manager

(Title of FDOT Representative)



Electronically signed within SWEPT
on February 27, 2026 8:44:08 AM EST
(electronic signature on file)

Link to Public Hearing Transcript

1 - [43798312101-CE2-D5-blic_Hearing_Transcript_of_In-Person_Session-2026-0216.pdf](#)

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FLORIDA DEPARTMENT OF TRANSPORTATION

PUBLIC HEARING

VIRTUAL

**Project Development and
Environment (PD&E) Study**

5:30 p.m. to 6:38 p.m.

Reported by:

Brett Rickel, Court Reporter

American Court Reporters
407.896.1813

* * * * *

P R O C E E D I N G S

January 20, 2026

5:30 p.m.

RECORDING: Welcome to the State Road 524 Project Development and Environment, or PD&E Study Public Hearing. Financial Project ID or FPID No. 437983-1. Efficient Transportation Decision Making, or ETDM No. 14321.

The three primary components of tonight's hearing are: First, the open house. Second, a presentation. Third, a formal comment period following the presentation, where you will have the opportunity to provide oral statements.

This is the Open House portion, where you're invited to view the project materials and provide comments in writing. For online participants, the GoToWebinar controls should be visible in the upper-right corner and across the bottom of your computer screen. If joining GoToWebinar on your mobile device, simply tap the screen to see the toolbar. The blue arrows point to where you'll find the question box. You can type a comment or question into the question box on your desktop or mobile app.

If joining from your computer, you may

1 download handouts by clicking on the icon as
2 shown by the red arrow. Click on the file name
3 to download. You may also visit the project
4 website at www.cflroads.com/project/437983-1 to
5 view the hearing materials, project documents,
6 or submit written comments or questions. All
7 questions will be responded to after the
8 hearing.

9 If you happen to experience a technical
10 issue during this hearing, please type the issue
11 in the questions box on the control panel on
12 GoToWebinar, or send an email to
13 carolyn.fitzwilliam@dot.state.fl.us. You may
14 also call 386-943-5221. Staff will do their
15 best to assist you.

16 At this time, we invite you to review the
17 hearing materials and submit any written
18 questions or comments you may have. The
19 presentation will begin at 6:00 p.m. and will be
20 followed by a formal comment period. If you
21 wish to make a verbal comment during the formal
22 comment period, you may type, I wish to speak,
23 in the questions box on the control panel at any
24 time.

25 After the presentation, we will call on

1 participants to provide their comments in the
2 order in which the request was received.

3 Thank you.

4 (Recording played again.)

5 MR. FONTANELLI: Good evening and welcome
6 to the public hearing for the State Road 524
7 Project Development and Environment, or PD&E
8 Study. Thanks for taking the time to join us
9 tonight. My name is Joseph Fontanelli and I'm
10 the Project Development Supervisor with the
11 Florida Department of Transportation.

12 At this time, we'd like to recognize any
13 Federal, State, County, or City officials who
14 may be present tonight. Are there any officials
15 that would like to be recognized? If so, please
16 enter your name in the question box in the
17 control panel in the GoToWebinar. While we wait
18 on our information, I have a few additional
19 things to mention.

20 This hearing is being held to provide you
21 with the opportunity to provide feedback on this
22 project. I also want to mention that tonight's
23 hearing is being recorded. The presentation
24 will provide information on the project and
25 FDOT's plans to improve safety and enhance

1 operations on State Road 524 from Friday Road
2 South to Industry Road, in Brevard County. The
3 Financial Project Identification No. is
4 437983-1-22-01.

5 We encourage your feedback. We're going to
6 provide you with several ways to provide your
7 input tonight. All questions and comments will
8 become part of the public hearing record.

9 If there are any officials present, please
10 let us know. (No response.)

11 At this time, we have not received any
12 official's words or names, so we begin our
13 presentation.

14 Thank you for attending. We'll now begin.

15 RECORDING: Information is being provided
16 in multiple ways to allow the community to
17 receive information about the project and
18 provide input. This hearing is being conducted
19 virtually on January 20, 2026, through
20 GoToWebinar, and in-person on January 22, 2026.

21 All hearing materials, including the
22 presentation, are available on the project
23 website at www.cflroads.com/project/437983-1.
24 For online participants, the GoToWebinar
25 controls should be visible in the upper-right

1 corner and across the bottom of your computer
2 screen. If joining GoToWebinar on your mobile
3 device, simply tap the screen to see the
4 toolbar. The blue arrows point to where you'll
5 find the question box. You can type a comment
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12 technical issue during this hearing, please type
13 the issue in the questions box on the control
14 panel on GoToWebinar, or send an email to
15 carolyn.fitzwilliam@dot.state.fl.us. You may
16 also call 386-943-5221. Staff will do their
17 best to assist you.

18 The purpose of tonight's public hearing is
19 to share information with the general public
20 about the proposed improvement, its conceptual
21 design, all alternatives under study and the
22 potential beneficial and adverse social,
23 economic and environmental impacts upon the
24 community. The public hearing also serves as an
25 official forum providing an opportunity for

1 members of the public to express their opinions
2 regarding the project.

3 There are three primary components to
4 tonight's hearing. First, the Open House, which
5 occurred prior to this presentation, where you
6 were invited to view the project materials and
7 provide your comments in writing. Second, this
8 presentation, which will explain the project
9 purpose and need, study alternatives, potential
10 impacts both beneficial and adverse, and
11 proposed methods to mitigate adverse project
12 impacts. And third, a formal comment period
13 following this presentation, where you will have
14 the opportunity to provide oral statements, or
15 you may provide your comments in writing.

16 This PD&E Study is located within the City
17 of Cocoa in Brevard County, and it involves
18 improvements along State Road 524 from Friday
19 Road South to Industry Road. The project also
20 involves modifications to the I-95 interchange.
21 The length of the project is approximately 3.15
22 miles.

23 The purpose and need of this project is to
24 accommodate existing and future travel demand,
25 and to enhance safety along State Road 524 for

1 vehicles and for bicycles and pedestrians. The
2 project development process involves five
3 phases: Planning, project development and
4 environment, or PD&E, design, right-of-way
5 acquisition and construction. We are currently
6 in the PD&E Study phase.

7 For this project, the PD&E and design
8 phases are overlapping to expedite the process.
9 Looking ahead, the right-of-way phase and the
10 construction phase are not currently funded. A
11 project kickoff newsletter was sent to the
12 public in March 2019.

13 The study team coordinated throughout with
14 the City of Cocoa, Space Coast Transportation
15 Planning Organization, and Brevard County on
16 various elements of the project, as well as
17 meeting with local stakeholders, including
18 homeowners' groups, recently built apartment
19 complexes and other area groups to get the word
20 out about the project and gain public input.

21 An alternatives public meeting was also
22 held on May 4, 2021, where the team presented
23 alternatives which led to the preferred
24 alternative being presented at this public
25 hearing. The public provided input on various

1 intersection types and typical section elements,
2 including median and lane widths and bicycle
3 pedestrian accommodations. As project details
4 were refined, other elements, including the
5 proposed interchange improvements at I-95,
6 proposed noise barriers and proposed
7 intersection improvements have been coordinated
8 with stakeholders in advance of this public
9 hearing.

10 This public hearing is another opportunity
11 for public input on the preferred alternative
12 being presented. This project has been
13 identified and shown in the cost feasible
14 portion of the 2045 Long Range Transportation
15 Plan of the Space Coast Transportation Planning
16 Organization, or TPO. The design phase of this
17 project is also included in the TPO's
18 Transportation Improvement Program for fiscal
19 years 2026 to 2030.

20 The project area has experienced growth in
21 recent years. What used to be a rural area is
22 now home to a number of residential communities
23 and commercial developments. Congestion is
24 expected to continue to increase if no
25 improvements are made. The I-95 interchange

1 will continue to be a heavily traveled section
2 of the project. Within the interchange area,
3 traffic volume is projected to reach nearly
4 40,000 vehicles per day in 2045. East of the
5 interchange, the volume will reach 22,000 to
6 32,000 vehicles by 2045, which is significantly
7 more than the existing two-lane roadway and
8 interchange can handle.

9 There have been 297 crashes recorded in the
10 project area between 2019 and 2023, an average
11 of 59 per year. Rear-end crashes were the most
12 common. This type of crash is directly linked
13 to congestion. Left turn crashes and head-on
14 crashes were also observed. Two fatal crashes
15 and 93 injury crashes were reported during the
16 same period.

17 It should be noted that the existing State
18 Road 524 has a lack of pedestrian and bicycle
19 features. Since future traffic volumes are
20 projected to increase substantially, crashes are
21 also anticipated to increase if no improvements
22 are made to this corridor.

23 In this study, we are evaluating a no-build
24 alternative and one build alternative which
25 offers various improvements. The no-build

1 alternative will maintain the existing roadway
2 lanes and interchange configuration. Although
3 it does not address the project's purpose and
4 need, the no-build alternative was carried
5 through the project evaluation as a baseline for
6 comparison purposes, as per the PD&E Study
7 guidelines.

8 The build alternative was developed based
9 on an evaluation of existing data, including
10 crash history, roadway conditions, intersection
11 operations and future traffic demands. The
12 build alternative involves widening State Road
13 524 from two lanes to four lanes, with two lanes
14 in each direction, and a raised grass median
15 separating the directions of traffic. A shared
16 use path is proposed on both sides of the
17 roadway to accommodate bicycle and pedestrian
18 users.

19 The I-95 interchange will be reconfigured
20 to improve traffic operations and safety and the
21 existing I-95 bridges over State Road 524 will
22 be replaced. Intersections throughout the
23 project will be upgraded with turn lanes and the
24 intersections at Cox Road and London Boulevard
25 will be reconfigured as roundabouts to enhance

1 safety by reducing vehicle conflicts and
2 encouraging lower speeds and improving traffic
3 operations.

4 The I-95 interchange is proposed to be
5 converted to a diverging diamond interchange, or
6 DDI, and the signals will operate in
7 coordination with the adjacent intersections at
8 Friday Road South and North, which will also be
9 signalized. The DDI will provide improved
10 traffic operation, reduced delay and reduce the
11 number and severity of crashes at the
12 interchange.

13 These graphics show what State Road 524
14 will look like under the preferred alternative.
15 The road will be widened from two to four lanes,
16 with curb and gutter directing stormwater to a
17 closed drainage system, with underground inlets
18 and pipes conveying to off-site ponds.

19 From Friday Road North to Cox Road, and
20 then from London Boulevard to Industry Road, the
21 median width will be 22 feet wide. From Cox
22 Road to London Boulevard, the median width will
23 vary from 22 to 60 feet. The varying width will
24 allow the lanes to gently shift to encourage
25 lower speeds. In addition, all lanes will be 11

1 feet wide also to encourage lower speeds, except
2 from Friday Road to Cox Road, where the outside
3 lanes will be 12 feet wide to accommodate
4 heavier truck movements to and from the I-95
5 interchange. Fourteen-foot-wide shared use
6 paths will be provided on both sides of the
7 roadway throughout the project.

8 Intersections will be improved and access
9 will be managed within FDOT Access Management
10 Guidelines to minimize vehicle conflicts. All
11 roadway improvements are proposed within the
12 existing right-of-way except at Cox Road and
13 London Boulevard intersections where minor
14 right-of-way, in form of corner clipping, will
15 be needed to accommodate the intersections.

16 The build alternative was selected as the
17 preferred alternative for the following reasons:
18 It enhances safety by adding a raised grass
19 median which separate opposing lanes of traffic
20 and thereby reducing the chances of motor
21 vehicle crashes and improving intersections
22 throughout the corridor. Also, the build
23 alternative adds shared use paths that enhance
24 the safety and travel of pedestrians and
25 bicyclists.

1 The proposed improvements in the build
2 alternative increase capacity along State Road
3 524 and at the I-95 interchange, thereby
4 accommodating future traffic growth in the
5 project area.

6 Additionally, public input was considered
7 in the selection of the preferred alternative.
8 Various intersection concepts were evaluated in
9 this project utilizing FDOT's intersection
10 control evaluation process. As a result of the
11 evaluation, a traditional signal control will
12 remain at the Walmart Distribution entrance. A
13 modern roundabout is proposed at the Cox Road
14 intersection and directional median openings are
15 proposed at the Pinion Drive and Lance Boulevard
16 intersections.

17 Continuing east, a modern roundabout is
18 proposed at London Boulevard, a directional
19 median opening is proposed at Coventry Court,
20 and traditional signal control will remain at
21 the Cirrus Drive and Industry Road
22 intersections.

23 There are various benefits of the proposed
24 modern roundabouts in contrast with traditional
25 traffic circles. Unlike traffic circles, modern

1 roundabouts are designed with a focus on safety
2 and traffic flow efficiency. Modern roundabouts
3 function through yielding, thus eliminating
4 full-stop conditions and delays associated with
5 traffic lights. Connectivity and safety
6 benefits include allowing continuous traffic
7 flow where the drivers keep moving through the
8 intersection without stopping. This can
9 increase intersection capacity up to fifty
10 percent.

11 Modern roundabouts can result in less
12 severe crashes when compared to regular
13 intersections due to the reduced conflict points
14 and the elimination of head-on and high-speed
15 right-angle collisions. Additionally,
16 roundabouts have ninety percent fewer fatalities
17 and seventy-five percent fewer injuries due to
18 the removal of right angle, head-on, and T-bone
19 conflict movements. They can also reduce up to
20 forty percent of pedestrian and bicycle crashes.
21 Benefits also include maintaining functionality
22 during power outages and lower construction and
23 maintenance costs when compared to traditional
24 signalized intersections.

25 This PD&E Study is evaluating potential

1 impacts to the social and economic, cultural,
2 natural, and physical environments associated
3 with each alternative. The project has been
4 evaluated with respect to the Presidential
5 Executive Order 11990, entitled Protection of
6 Wetlands, and Executive Order 11988, entitled
7 Floodplain Management and Protection.

8 Avoidance and minimization of impacts to
9 these features were considered in the selection
10 of the preferred alternative. A drainage
11 analysis was conducted to identify and assess
12 potential locations for stormwater ponds that
13 would provide both water quality treatment and
14 flood control along the corridor. The four
15 selected ponds are: Pond 1A near I-95, pond 2F
16 at Cox Road and Ponds 3A and 3B located near
17 Industry Road. A closed drainage system is
18 proposed and no floodplain compensation site is
19 required.

20 The preferred alternative adds a median
21 along State Road 524 from Friday Road South to
22 Industry Road. It maintains the locations of
23 all existing intersections and provides median
24 openings at several locations according to the
25 Access Management Standards. The Current Access

1 Management Classification is proposed to change
2 from Type 3 to Type 5 between Friday Road South
3 and Friday Road North. The existing
4 non-restrictive classification of four is
5 proposed to be changed to Classification 3 from
6 Friday Road north to Cox Road and to
7 Classification 5 from Cox Road to Industry Road.
8 Both classification types 3 and 5 include a
9 restrictive median. The concept plans show the
10 access provided at each existing intersection.

11 This project is being developed in
12 accordance with Section 335.199 of the Florida
13 Statutes, requiring FDOT to notify all affected
14 property owners, municipalities and counties of
15 a proposed project that will close or modify an
16 existing access to an abutting property owner at
17 least 180 days before the design is finalized.

18 Right-of-way acquisition is needed for
19 intersection improvements and stormwater
20 management ponds. We anticipate 14 parcels will
21 be impacted, totaling 8.29 acres. One of the
22 unavoidable consequences on a project such as
23 this is the necessary relocation of residences
24 or businesses. On this project, we anticipate
25 the relocation of one business and no

1 residential relocations.

2 All right-of-way acquisition will be
3 conducted in accordance with Florida Statutes
4 339.09 and 421.55 and the Federal Uniform
5 Relocation Assistance and Real Property
6 Acquisition Policies Act of 1970, commonly known
7 as the Uniform Act. If you are required to make
8 any type of move as a result of a Department of
9 Transportation Project, you can expect to be
10 treated in a fair and helpful manner and in
11 compliance with the Uniform Relocation
12 Assistance Act.

13 If a move is required, you will be
14 contacted by an appraiser who will inspect your
15 property. We encourage you to be present during
16 the inspection and provide information about the
17 value of your property. You may also be
18 eligible for relocation advisory services and
19 payment benefits. If you are being moved and
20 you are unsatisfied with the Department's
21 determination of your eligibility for payment or
22 the amount of that payment, you may appeal that
23 determination. You will be promptly furnished
24 necessary forms and notified of the procedures
25 to be followed in making that appeal. A special

1 word of caution: If you move before you receive
2 notification of the relocation benefits that you
3 might be entitled to, your benefits may be
4 jeopardized.

5 For those attending virtually, you may
6 reach out to the FDOT project manager who will
7 direct your request to the appropriate
8 relocation specialists. An evaluation matrix,
9 shown here, compares the benefits, impacts, and
10 costs associated with each alternative. The
11 build alternative meets the purpose and need by
12 accommodating future traffic demand, improving
13 safety and improving bicycle and pedestrian
14 facilities. The no-build alternative would not
15 meet the project's purpose and need.

16 Overall, acquisition of right-of-way from
17 14 parcels for a total of 8.29 acres will be
18 needed to accommodate stormwater ponds and the
19 roadway improvements. The build alternative
20 will not cause any residential relocation, and
21 one of the pond sites will require a business
22 relocation. There are not anticipated impacts
23 to historic and cultural resources, nor from any
24 recreational resources.

25 The proposed roadway widening and pond

1 sites would result in direct permanent impacts
2 to 7.36 acres of wetlands. Most of these
3 wetlands are located within the existing roadway
4 right-of-way. Twenty-seven plant species, 16
5 listed wildlife species and one candidate
6 wildlife species have the potential to occur
7 within the project area. However, the
8 likelihood of the project's potential impact of
9 these species is low.

10 Seven potential contamination sites
11 adjacent to the project have a medium or high
12 likelihood of being affected by the build
13 alternate. There are 63 noise-sensitive sites
14 adjacent to the project, including single-family
15 and multi-family homes. Analysis of the
16 predicted noise levels associated with the build
17 alternative indicates that the implementation of
18 the build alternative would cause substantial
19 noise level increases and noise walls are
20 recommended at three locations as shown on the
21 concept plans. Implementation of the build
22 alternative will likely result in relocations to
23 some of the existing utilities.

24 The estimated total project cost is \$170.4
25 million and is split as follows. The estimated

1 cost of right-of-way is \$17.3 million, wetland
2 mitigation is \$300,000, construction is \$138.9
3 million and construction engineering and
4 inspection is \$13.9 million.

5 The PD&E Study is expected to be completed
6 by the middle of 2026. Public engagement
7 activities and opportunities for the public
8 throughout the PD&E Study have included
9 individual stakeholder coordination meetings, a
10 public kickoff newsletter, an alternatives
11 public information meeting and this public
12 hearing.

13 Design has started and is being conducted
14 concurrently with this PD&E Study. The next
15 step after today's public hearing is to
16 incorporate your input into our decision-making
17 process. After the comment period closes and
18 your input has been considered, a decision will
19 be made, and the final PD&E documents will be
20 sent to the FDOT Office of Environmental
21 Management for approval.

22 To submit a comment or question online,
23 please type the comment or question in the
24 question box on GoToWebinar. Written comments
25 may also be submitted on the project website at

1 www.cflroads.com/project/437983-1.

2 To learn more about the project, go to
3 www.cflroads.com. Type the Project No. 437983-1
4 in the search box at the top right and click go.
5 Then click on the project name. Public hearing
6 materials are posted on the website now.

7 We encourage your input and feedback about
8 this project, and there are multiple ways for
9 you to participate. All public comments and
10 questions are part of the public hearing record,
11 and every method for providing public comments
12 and questions carries equal weight. While
13 comments and questions will be accepted at any
14 time, those submitted by February 2, 10 days
15 after the public hearing, will become part of
16 the project's public hearing record. All
17 questions will be responded to in writing
18 following the hearing.

19 You may also contact FDOT project manager
20 Maria Serrano-Acosta directly by email at
21 maria.serrano-acosta@dot.state.fl.us. Or by
22 U.S. mail at the Florida Department of
23 Transportation, 719 South Woodland Boulevard,
24 Mail Station 501 Deland, Florida 32720-6834.
25 You may also call the Project Manager at

1 386-943-5063 to provide verbal comments during
2 normal business hours. The contact information
3 is also available on the public hearing
4 notification that you may have received by mail.

5 Project documents are available for viewing
6 until February 2nd, 2026 at Catherine
7 Schweinsberg Rood Central Library, 308 Forest
8 Avenue, Cocoa, Florida, 32922, during the hours
9 of 9:00 a.m. to 6:00 p.m. on Monday and
10 Wednesday, 9:00 a.m. to 8:00 p.m. on Tuesday and
11 Thursday, 9:00 a.m. to 5:00 p.m. on Friday and
12 Saturday, 1:00 p.m. to 5:00 p.m. on Sunday. The
13 project documents are also available on the
14 project website at www.cflroads.com/project/437983-1.

15 This public hearing was advertised and is
16 being conducted in accordance with state and
17 federal requirements, including Title VI of the
18 Civil Rights Act of 1964. Public participation
19 is solicited without regard to race, color,
20 national origin, age, sex, religion, disability
21 or family status. Persons wishing to express
22 their concerns about Title VI may do so by
23 contacting Melissa McKinney, District Five Title
24 VI Coordinator, by mail at 719 South Woodland
25 Boulevard, MS-520, Deland, Florida 32720-6834,

1 by phone at 386-943-5077 or email
2 melissa.mckinney@dot.state.fl.us.

3 You may also contact Al Sanders, Interim
4 State Title VI Coordinator, by mail at 605
5 Suwannee Street, Mail Station 65, Tallahassee,
6 Florida, 32399-0450, by phone at 850-414-4764 or
7 email at aldrin.sanders@dot.state.fl.us. This
8 information is shown on a sign at the in-person
9 location, on the project website and in the
10 hearing notifications.

11 The public hearing was advertised in the
12 Florida Administrative Register, on FDOT's
13 Public Notices website, the project website and
14 in the local newspaper. In addition, adjacent
15 property owners, interested individuals, elected
16 and appointed officials and government agencies
17 were also notified about this public meeting.
18 This public hearing was advertised consistent
19 with the federal and state requirements shown on
20 the slide.

21 The environmental review, consultation and
22 other actions required by applicable federal
23 environmental laws for this project are being or
24 have been carried out by FDOT pursuant to 23
25 U.S.C 327 and a Memorandum of Understanding,

1 dated May 26, 2022, and executed by the Federal
2 Highway Administration and FDOT.

3 The next step is to incorporate your input
4 on this public hearing into our decision-making
5 process. After the comment period closes and
6 your input has been considered, a decision will
7 be made, and the final PD&E document will be
8 approved. This project has and will continue to
9 comply with all applicable state and federal
10 rules and regulations.

11 This concludes the presentation. Thank you
12 for attending this public hearing and providing
13 your input on this project.

14 MR. FONTANELLI: Thank you.

15 Before we begin the formal public comment
16 period, I'd like to recognize Councilwoman
17 Lorraine Koss from the City of Cocoa. Thank you
18 for being in attendance tonight.

19 We will now enter the formal public comment
20 period for this hearing. Anyone who wishes to
21 make a verbal statement regarding the project
22 will now have the opportunity to speak. Please
23 know that tonight's public hearing is being
24 recorded. All questions, including those
25 received on the GoToWebinar and comments will

1 become part of the public hearing record. And
2 we will respond to all questions in writing
3 after this hearing.

4 You can ask to speak using the GoToWebinar
5 control panel by typing your name and I wish to
6 speak in the questions box on the control panel.
7 When it is your turn, we'll call your name and
8 your microphone will be unmuted. You may also
9 call Maria Serrano-Acosta, the FDOT Project
10 Manager, at 386-943-5063 after this public
11 hearing.

12 We will now begin hearing online
13 participants who have requested to speak. When
14 your name is called, we will need you to unmute
15 your microphone using the GoToWebinar control
16 panel as shown on the slide. If the microphone
17 is orange, that means you need to unmute
18 yourself. If the microphone is green, it means
19 your microphone is unmuted, and you may speak at
20 any time.

21 Please state your name and address before
22 making your comment. If you represent an
23 organization, a municipality or other public
24 body, please provide information as well.

25 To ensure all who wish to speak today are

1 able to, speakers will have a maximum of three
2 minutes to make a statement and we will respond
3 to all questions in writing after the hearing.
4 The timer you see on the screen reflects each
5 speaker's remaining time.

6 We would now like to open up to our first
7 speaker. Does anyone wish to speak? Again, we
8 would ask if anyone would like to speak. Please
9 state your name and I wish to speak now in the
10 control panel.

11 Does anyone wish to speak? Okay.

12 Well, we'll now close the formal comment
13 period -- one second. I apologize. Okay. We
14 did get a name.

15 Mr. Tom Kennelly. Please excuse me if I
16 mispronounce your name. Mr. Tom Kennelly.

17 MR. KENNELLY: Okay. Sorry about that.
18 I'm curious with -- and I wrote this question
19 in, but -- with all of the construction that the
20 City of Cocoa has allowed along 524, who is
21 picking up the bill to widen 524? It should
22 have been some of the developers along 524, but
23 I'm not hearing or I don't know that they paid
24 impact fees to upgrade 524.

25 End of comment.

1 MR. FONTANELLI: Mr. Kennelly, thank you
2 for your comment. We'll look into it. We'll
3 respond to everything in writing. So we will
4 respond to you in writing as we research your
5 question. Thank you.

6 MR. KENNELLY: Thank you.

7 MR. FONTANELLI: Does anyone else wish to
8 speak? We do not see anybody who says they wish
9 to speak. We are receiving questions on the
10 GoToWebinar.

11 Standby.

12 Ms. Susan Frank? (No response.)

13 Ms. Susan Frank? (No response.)

14 MS. FITZWILLIAM: Ms. Frank, you should be
15 able to unmute now.

16 MR. FONTANELLI: Ms. Frank?

17 MS. FITZWILLIAM: I see that you're
18 unmuted, but we cannot hear you. Apologies for
19 that.

20 MR. FONTANELLI: And please let us try to
21 get Ms. Frank so she could speak. One moment.

22 Thank you, Ms. Frank. We apologize for the
23 sound issue, but please let us know in writing
24 and we will respond to you in writing as well.
25 Thank you.

1 Does anyone else wish to speak? Anyone
2 else wish to speak? Anyone else? (No response.)

3 They're receiving no other requests. We'll
4 move on. Please give us one second.

5 All right. Well, thank you for your time.
6 On behalf of the Florida Department of
7 Transportation, we want to thank you for
8 attending this public hearing and providing your
9 input on this project. If you have any comments
10 or questions after the hearing, please submit
11 them by February 2nd, 2026.

12 It is now 6:38 p.m. and I hereby officially
13 close the public hearing for the State Road 524
14 PD&E Study. Have a good evening. Be safe.

15 Thank you.

16 (The meeting was concluded at 6:38 p.m.)

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C E R T I F I C A T E

STATE OF FLORIDA)
COUNTY OF POLK)

I, BRETT S. RICKEL, Court Reporter, certify that I was authorized to report and transcribe the aforementioned Public Hearing and that the transcript is a true and complete record of the recording.

I further certify that I am not a relative, employee, attorney or counsel of any of the parties, nor am I financially interested in the outcome of the foregoing action.

DATED this 5th day of February, 2026.

Brett S. Rickel

BRETT S. RICKEL, Court Reporter
Notary Public, State of Florida
(electronic signature)

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FLORIDA DEPARTMENT OF TRANSPORTATION

PUBLIC HEARING

SR 524

Public Hearing Presentation

**Project Development and
Environment (PD&E) Study**

**Holiday Inns & Suites Cocoa
Space Coast Convention Center
301 Tucker Lane
Cocoa, Florida**

5:00 p.m. to 7:17 p.m.

**Reported by:
Cindy R. Green, Court Reporter**

American Court Reporters
407.896.1813

* * * * *

P U B L I C H E A R I N G

January 22, 2026

6:00 p.m.

(The FDOT Public Hearing was called to order, after which the following took place.)

MR. FONTANELLI: Good evening. All right, friends, we're about to begin the formal portion of this presentation.

So I still hear a lot of our friends over next door, so we'll give them a courtesy one more minute, but then we'll jump into it to stay on stay on schedule. I want to be respectful of everybody's time this evening. So just give me one more minute and then we'll kick this thing off. (Pause.)

All righty then. Good evening, everyone. Good evening. Good evening, everyone. All right. Good evening and welcome to the public hearing for State Road 524 Project Development and Environment, or what we call PD&E study.

I really appreciate everybody coming out tonight and thank you for joining us tonight on the interaction. It's been great. Thank you.

My name is Joseph Fontanelli. I'm the Project Development Supervisor with the Florida

1 Department of Transportation.

2 This hearing is being held to provide you
3 with the opportunity to provide feedback on this
4 project. I also want to mention that this
5 hearing tonight is being recorded. The
6 presentation we'll provide for you is on the
7 project and FDOT's plans to improve safety and
8 enhance operations on State Road 524 from Friday
9 Road South to Industry Road in Brevard County.

10 The project information number is
11 437983-1-2201 and is on the handouts that you
12 guys received. We do encourage your feedback
13 and we're going to provide you with several ways
14 to provide your input tonight. All questions
15 and comments will become part of the public
16 hearing record.

17 At this time, I would like to recognize any
18 federal, state, county or city officials who may
19 be present tonight. If any of the officials
20 would like to be recognized, we have the
21 microphone here. You can please identify who
22 you are and who you represent. Is there anyone
23 who would like to be recognized this evening?

24 MS. DELANEY: Good evening, everybody. I'm
25 Katie Delaney. I'm the County Commissioner for

1 District 1. And thank you all so much for
2 coming out tonight and being a part of this and
3 giving us your comments. And thank you guys for
4 having this meeting.

5 MR. FONTANELLI: Thank you, Commissioner
6 Delaney. Thanks for being here.

7 Is there anyone else who'd like to be
8 recognized? Any city, state, county official.

9 We do have Ms. Abby Morgan here from the
10 city of Cocoa Beach.

11 MR. STOLL: I'll put a bull's eye on.

12 MR. FONTANELLI: Yes, sir.

13 MR. STOLL: My name is Gregg Stoll. I am
14 with Space Coast TPO, where I serve as a citizen
15 advisor to the City of Cocoa.

16 MR. FONTANELLI: Thank you.

17 Is there any other official who would like
18 to be recognized. (No response.)

19 We will now begin the presentation and
20 following the presentation, we will have a
21 formal comment period. Let's begin.

22 VIDEO: Information is being provided in
23 multiple ways to allow the community to receive
24 information about the project and provide input.
25 This hearing is being conducted virtually on

1 January 20th, 2026, through GoToWebinar and
2 in-person on January 22nd, 2026.

3 All hearing materials, including the
4 presentation, are available on the project
5 website at www.cflroads.com/project/437983-1.

6 The purpose of tonight's public hearing is
7 to share information with the general public
8 about the proposed improvement, its conceptual
9 design, all alternatives under study, and the
10 potential beneficial and adverse social,
11 economic, and environmental impacts upon the
12 community.

13 The public hearing also serves as an
14 official forum providing an opportunity for
15 members of the public to express their opinions
16 regarding the project. There are three primary
17 components to tonight's hearing.

18 First, the open house, which occurred prior
19 to this presentation, where you were invited to
20 view the project displays and speak directly
21 with the project team, and provide your comments
22 in writing or to the court reporter.

23 Second, this presentation, which will
24 explain the project purpose and need, study
25 alternatives, potential impacts, both beneficial

1 and adverse, and proposed methods to mitigate
2 adverse project impacts.

3 And third, a formal comment period
4 following this presentation, where you will have
5 the opportunity to provide oral statements at
6 the microphone, or you may provide your comments
7 directly to the court reporter or in writing.

8 This PD&E study is located within the city
9 of Cocoa in Brevard County, and it involves
10 improvements along State Road 524 from Friday
11 Road South to Industry Road. The project also
12 involves modifications to the I-95 interchange.
13 The length of the project is approximately 3.15
14 miles. The purpose and need of this project is
15 to accommodate existing and future travel demand
16 and to enhance safety along State Road 524 for
17 vehicles and for bicycles and pedestrians.

18 The project development process involves
19 five phases: Planning, Project Development and
20 Environment, or PD&E, Design, Right-of-Way
21 Acquisition, and Construction. We are currently
22 in the PD&E study phase. For this project, the
23 PD&E and design phases are overlapping to
24 expedite the process. Looking ahead, the
25 right-of-way phase and the construction phase

1 are not currently funded.

2 A project kickoff newsletter was sent to
3 the public in March 2019. The study team
4 coordinated throughout with the City of Cocoa,
5 Space Coast Transportation Planning
6 Organization, and Brevard County on various
7 elements of the project, as well as meeting with
8 local stakeholders, including homeowners groups,
9 recently built apartment complexes, and other
10 area groups to get the word out about the
11 project and gain public input.

12 An alternatives public meeting was also
13 held on May 4th, 2021, where the team presented
14 alternatives which led to the preferred
15 alternative being presented at this public
16 hearing. The public provided input on various
17 intersection types and typical section elements,
18 including median and lane widths, and bicycle
19 pedestrian accommodations. As project details
20 were refined, other elements, including the
21 proposed interchange improvements at I-95,
22 proposed noise barriers, and proposed
23 intersection improvements have been coordinated
24 with stakeholders in advance of this public
25 hearing.

1 This public hearing is another opportunity
2 for public input on the preferred alternative
3 being presented.

4 This project has been identified and shown
5 in the cost physical portion of the 2045 Long
6 Branch Transportation Plan of the Space Coast
7 Transportation Planning Organization, or TPO.
8 The design phase of this project is also
9 included in the TPO's Transportation Improvement
10 Program for fiscal years 2026 to 2030.

11 The project area has experienced growth in
12 recent years. What used to be a rural area is
13 now home to a number of residential communities
14 and commercial developments. Congestion is
15 expected to continue to increase if no
16 improvements are made.

17 The I-95 interchange will continue to be a
18 heavily traveled section of the project. Within
19 the interchange area, traffic volume is
20 projected to reach nearly 40,000 vehicles per
21 day in 2045.

22 East of the interchange, the volume will
23 reach 22,000 to 32,000 vehicles by 2045, which
24 is significantly more than the existing two-lane
25 roadway and interchange can handle.

1 There have been 297 crashes reported in the
2 project area between 2019 and 2023, an average
3 of 59 per year. Rear-end crashes were the most
4 common. This type of crash is directly linked
5 to congestion. Left turn crashes and head-on
6 crashes were also observed. Two fatal crashes
7 and 93 injury crashes were reported during the
8 same period.

9 It should be noted that the existing State
10 Road 524 has a lack of pedestrian and bicycle
11 features. Since future traffic volumes are
12 projected to increase substantially, crashes are
13 also anticipated to increase if no improvements
14 are made to this corridor.

15 In this study, we are evaluating a no-build
16 alternative and one build alternative which
17 offers various improvements.

18 The no-build alternative will maintain the
19 existing roadway lanes and interchange
20 configuration.

21 Although it does not address the project's
22 purpose and need, the no-build alternative was
23 carried through the project evaluation as a
24 baseline for comparison purposes as per the PD&E
25 study guidelines.

1 The build alternative was developed based
2 on an evaluation of existing data, including
3 crash history, roadway conditions, intersection
4 operations, and future traffic demands.

5 The build alternative involves widening
6 State Road 524 from two lanes to four lanes,
7 with two lanes in each direction and a raised
8 grass medium separating the directions of
9 traffic.

10 A shared use path is proposed on both sides
11 of the roadway to accommodate bicycle and
12 pedestrian users.

13 The I-95 interchange will be reconfigured
14 to improve traffic operations and safety and the
15 existing I-95 bridges over State Road 524 will
16 be replaced.

17 Intersections throughout the project will
18 be upgraded with turn lanes and the
19 intersections at Cox Road and London Boulevard
20 will be reconfigured as roundabouts to enhance
21 safety by reducing vehicle conflicts,
22 encouraging lower speeds, and improving traffic
23 operations.

24 The I-95 interchange is proposed to be
25 converted to a diverging diamond interchange, or

1 DDI, and the signals will operate in
2 coordination with the adjacent intersections at
3 Friday Road south and north, which will also be
4 signalized. The DPI will provide improved
5 traffic operation, reduced delay, and reduce the
6 number and severity of crashes at the
7 interchange.

8 These graphics show what State Road 524
9 will look like under the preferred alternative.
10 The road will be widened from two to four lanes,
11 with curb and gutter directing stormwater to a
12 closed drainage system with underground units
13 and pipes conveying to offsite ponds.

14 From Friday Road north to Cox Road and then
15 from London Boulevard to Industry Road, the
16 median width will be 22-feet wide. From Cox
17 Road to London Boulevard, the median width will
18 vary from 22 to 60 feet.

19 The varying width will allow the lanes to
20 gently shift to encourage lower speeds. In
21 addition, all lanes will be 11-feet wide also to
22 encourage lower speeds, except from Friday Road
23 to Cox Road where the outside lanes will be
24 12-feet wide to accommodate heavier truck
25 movements to and from the I-95 interchange.

1 Fourteen-foot-wide shared use paths will be
2 provided on both sides of the roadway throughout
3 the project.

4 Intersections will be improved and access
5 will be managed within FDOT Access Management
6 Guidelines to minimize vehicle confidence.

7 All roadway improvements are proposed
8 within the existing right of way except at Cox
9 Road and London Boulevard intersections where
10 minor right of way, in form of corner clipping
11 will be needed to accommodate the intersections.

12 The build alternative was selected as the
13 preferred alternative for the following reasons.

14 It enhances safety by adding a raised grass
15 median which separate opposing lanes of traffic
16 and thereby reducing the chances of motor
17 vehicle crashes and improving intersections
18 throughout the corridor. Also, the build
19 alternative adds shared use paths that enhance
20 the safety and travel of pedestrians and
21 bicyclists.

22 The proposed improvements in the build
23 alternative increase capacity along State Road
24 524 and at the I-95 interchange, thereby
25 accommodating future traffic road in the project

1 area.

2 Additionally, public input was considered
3 in the selection of the preferred alternative.

4 Various intersection concepts were
5 evaluated in this project utilizing FDOT's
6 intersection control evaluation process.

7 As a result of the evaluation, a
8 traditional signal control will remain at the
9 Walmart Distribution Entrance.

10 A modern roundabout is proposed at the Cox
11 Road intersection. And directional median
12 openings are proposed at the Pinyon Drive and
13 Lance Boulevard intersections.

14 Continuing east, a modern roundabout is
15 proposed at London Boulevard, a directional
16 median opening is proposed at Coventry Court,
17 and traditional signal control will remain at
18 the Cirrus Drive and Industry Road
19 intersections.

20 There are various benefits of the proposed
21 modern roundabouts in contrast with traditional
22 traffic circles. Unlike traffic circles, modern
23 roundabouts are designed with a focus on safety
24 and traffic flow efficiency. Modern roundabouts
25 function through yielding, thus eliminating

1 full-stop conditions and delays associated with
2 traffic lights.

3 Connectivity and safety benefits include
4 allowing continuous traffic flow where the
5 drivers keep moving through the intersection
6 without stopping. This can increase
7 intersection capacity up to 50 percent.

8 Modern roundabouts can result in less
9 severe crashes when compared to regular
10 intersections due to the reduced conflict points
11 and the elimination of head-on and high-speed
12 wide-angle collisions.

13 Additionally, roundabouts have 90 percent
14 fewer fatalities and 75 percent fewer injuries
15 due to the removal of right angle, head-on, and
16 T-bone conflict movements. They can also reduce
17 up to 40 percent of pedestrian and bicycle
18 crashes.

19 Benefits also include maintaining
20 functionality during power outages and lower
21 construction and maintenance costs when compared
22 to traditional signalized intersection.

23 This PD&D study is evaluating potential
24 impacts to the social and economic, cultural,
25 natural, and physical environments associated

1 with each alternative. The project has been
2 evaluated with respect to the Presidential
3 Executive Order 11990, entitled Protection of
4 Wetlands, and Executive Order 11988, entitled
5 Flood Plane Management and Protection.
6 Avoidance and minimization of impacts to these
7 features were considered in the selection of the
8 preferred alternative.

9 A drainage analysis was conducted to
10 identify and assess potential locations for
11 stormwater ponds that would provide both water
12 quality treatment and flood control along the
13 corridor. The four selected ponds are Pond 1A
14 near I-95, Pond 2F at Cox Road, and Pond 3A and
15 3B located near Industry Road. A closed
16 drainage system is proposed and no floodplain
17 compensation site is required.

18 The preferred alternative as a median along
19 State Road 524 from Friday Road South to
20 Industry Road. It maintains the locations of
21 all existing intersections and provides median
22 openings at several locations --

23 AUDIENCE MEMBER: Turn it up!

24 VIDEO: -- according to the Access
25 Management Standards. The current access

1 management classification is proposed to change
2 from Type 3 to Type 5. The preferred
3 alternative and the median along State Road 524
4 from Friday Road South to Industry Road. It
5 maintains the locations of all existing
6 intersections and provides median openings at
7 several locations according to the Access
8 Management Standards. The current Access
9 Management Classification is proposed to change
10 from Type 3 to Type 5 between Friday Road South
11 and Friday Road North. The existing
12 non-restrictive classification of 4 is proposed
13 to be changed to Classification 3 from Friday
14 Road North to Cox Road and to Classification 5
15 from Cox Road to Industry Road. Both
16 classification types 3 and 5 include the
17 restricted media.

18 The concept plans show the access provided
19 at each existing intersection. This project is
20 being developed in accordance with Section
21 335.199 of the Florida Statutes, requiring FDOT
22 to notify all affected property owners,
23 municipalities, and counties of a proposed
24 project that will close or modify an existing
25 access to an abutting property owner at least

1 180 days before the design is finalized.

2 Right-of-way acquisition is needed for
3 intersection improvements and stormwater
4 management pods. We anticipate 14 parcels will
5 be impacted totaling 8.29 acres.

6 One of the unavoidable consequences on a
7 project such as this is the necessary relocation
8 of residences or businesses. On this project,
9 we anticipate the relocation of one business and
10 no residential relocations. All right-of-way
11 acquisition will be conducted in accordance with
12 Florida Statutes 339.09 and 421.55 and the
13 Federal Uniform Relocation Assistance and Real
14 Property Acquisition Policies Act of 1970,
15 commonly known as the Uniform Act.

16 If you are required to make any type of
17 move as a result of a Department of
18 Transportation project, you can expect to be
19 treated in a fair and helpful manner and in
20 compliance with the Uniform Relocation
21 Assistance Act. If a move is required, you will
22 be contacted by an appraiser who will inspect
23 your property. We encourage you to be present
24 during the inspection and provide information
25 about the value of your property.

1 You may also be eligible for relocation
2 advisory services and payment benefits. If you
3 are being moved and you are unsatisfied with the
4 Department's determination of your eligibility
5 for payment or the amount of that payment, you
6 may appeal make it feel that determination.

7 You will be prompted to furnish the
8 necessary forms and notified of the procedures
9 to be followed in making that appeal.

10 A special word of caution. If you move
11 before you receive notifications of the
12 relocation benefits that you might be entitled
13 to, your benefits may be jeopardized. The
14 relocation specialists at the in-person hearing
15 will be happy to answer your questions and will
16 also furnish you with copies of relocation
17 assistance brochures.

18 An evaluation matrix shown here compares
19 the benefits, impacts, and costs associated with
20 each alternative.

21 The build alternative meets the purpose and
22 need by accommodating future traffic demands,
23 improving safety, and improving bicycle and
24 pedestrian facilities. The no-build alternative
25 would not meet the project's purpose and need.

1 Overall, acquisition of the right of way
2 and 14 parcels for a total of 8.29 acres will be
3 needed to accommodate stormwater bonds and the
4 roadway improvements. The build alternative
5 will not cause any residential relocation and
6 one of the pond sites will require a business
7 relocation.

8 There are not anticipated impacts to
9 historic and cultural resources.

10 AUDIENCE MEMBER: We can't hear it.

11 (Audio difficulties.)

12 AUDIENCE MEMBER: Turn it up!

13 AUDIENCE MEMBER: Stop the presentation.

14 (Pause to address audio difficulties.)

15 SPEAKER: All right. Sorry about that,
16 everyone. Thank you for your patience. We're
17 having some audio difficulties. So we're going
18 to go back to the slide with the evaluation
19 matrix where things started to get a little
20 lighter in volume. I'm going to read it. So
21 we'll be able to get through.

22 All right. An evaluation matrix shown here
23 compares the benefits, impacts, and costs
24 associated with each alternative. The build
25 alternative meets the purpose and need by

1 accommodating future traffic demand, improving
2 safety, and improving bicycle and pedestrian
3 facilities. No build alternative would not meet
4 the project's purpose and need.

5 Overall, acquisition of right-of-way from
6 14 parcels to a total of 8.29 acres will be
7 needed to accommodate stormwater ponds and the
8 roadway improvements. The build alternative
9 will not cause any residential relocations, and
10 one of the pond sites will require a business
11 relocation.

12 There are not anticipated impacts to
13 historic or cultural resources nor from any
14 recreational resources. The proposed roadway
15 widening in pond sites would result in direct
16 permanent impacts to 7.36 acres of wetlands.
17 Most of these wetlands are located within the
18 existing roadway right-of-way. 27 plant
19 species, 16 listed wildlife species, and one
20 candidate wildlife species have the potential to
21 occur within the project area. However, the
22 likelihood of the project's potential impact to
23 these species is low.

24 Seven potential contamination sites
25 adjacent to the project have a medium or high

1 likelihood of being affected by the build
2 alternative. There are 63 noise-sensitive sites
3 adjacent to the project, including single-family
4 and multifamily homes. Analysis of the
5 predicted noise levels associated with the build
6 alternative indicates that the implementation of
7 the build alternative would cause substantial
8 noise level increases and noise walls are
9 recommended at three locations as shown on the
10 concept plans.

11 Implementation of the build alternative
12 will likely result in relocations to some of the
13 existing utilities. The estimated total project
14 cost is 170.4 million and is split as follows.
15 The estimated cost for right away is 17.3
16 million. Wetland mitigation is 300,000.
17 Construction is 138.9 million and construction
18 engineering inspection, or CEI, is 13.9 million.

19 Next slide, please.

20 The PD&E study is expected to be completed
21 in the middle of 2026. Public engagement
22 activities and opportunities for the public
23 throughout the PD&E study have included
24 individual stakeholder coordination meetings, a
25 public kickoff newsletter, an alternatives

1 public information meeting, and this public
2 hearing. Design has already started and is
3 being conducted concurrently within the PD&E
4 study.

5 The next step after today's public hearing
6 is to incorporate your input into our
7 decision-making process. After the comment
8 period closes and your input has been
9 considered, a decision will be made and the
10 final PD&E documents will be sent to the FDOT
11 Office of Environmental Management for approval.

12 Next slide, please.

13 In-person attendees are encouraged to speak
14 with project team members to ask questions and
15 provide input.

16 To submit a comment for the public hearing
17 record, please complete a printed comment form
18 and return it to project staff. You may also
19 provide your comment directly to the court
20 reporter.

21 Next slide, please.

22 To learn more about the project, go to
23 www.cflroads.com. Type the project number
24 437983-1 in the search box at the top right and
25 click Go. Then click on the project name.

1 Public hearing materials are posted on the
2 website now.

3 Next slide, please.

4 We encourage your input and feedback about
5 the project, and there are multiple ways for you
6 to participate. All public comments and
7 questions are part of the public hearing record
8 and every method for providing public comments
9 and questions carries equal weight.

10 While comments and questions will be
11 accepted at any time, those submitted by
12 February 2nd, 10 days after the public hearing,
13 will become part of the project's public hearing
14 record.

15 All questions will be responded to in
16 writing following the hearing.

17 Next slide.

18 You may also contact the FDOT project
19 manager, Maria Serrano-Acosta, directly by
20 email, as shown on the screen.
21 Maria.Serrano-Acosta@dot.state.fl.us, or by US
22 Mail at the Florida Department of Transportation
23 719 South Woodland Boulevard, Mail Station 501,
24 Deland, Florida 32720-6834.

25 You may also call the project manager at

1 (386) 943-5063 to provide verbal comments during
2 normal business hours.

3 The contact information is also available
4 on the public hearing notification that you may
5 have received by mail.

6 Next slide, please.

7 Project documents are available for viewing
8 until February 2nd, 2026 at Catherine
9 Schweinberg Rood Central Library, 308 Forrest
10 Avenue, Cocoa Florida 32922, during the hours of
11 9:00 a.m. to 6:00 p.m., Monday through Wednesday
12 or 9:00 a.m. to 8:00 p.m. Tuesday and Thursday,
13 9:00 a.m. to 5:00 p.m. on Friday and Saturday,
14 and 1:00 p.m. to 5:00 p.m. on Sunday.

15 The project documents are also available on
16 the project website at
17 www.cflroads.com/project/437983-1.

18 Next slide, please.

19 The public hearing was advertised and is
20 being conducted in accordance with state and
21 federal requirements, including Title VI of the
22 Civil Rights Act of 1964. Public participation
23 is solicited without regard to race, color,
24 national origin, age, sex, religion, disability,
25 or family status.

1 Persons wishing to express their concerns
2 about Title VI may do so by contacting Melissa
3 McKinney, District 5 Title VI Coordinator by
4 mail at 719 South Woodland Boulevard, Mail
5 Station 520, Deland, Florida 32720-6834, by
6 phone at (386) 943-5077, or email Melissa
7 McKinney at DOT or Melissa.McKinney@dot.state.fl.us.

8 You may also contact Al Sanders, Interim
9 State Title VI coordinator, by mail at 605
10 Suwanee Street, Mail Station 65, Tallahassee,
11 Florida 32399-0450, or by phone at
12 (850) 414-4764, or email at
13 Aldrin.Sanders@dot.state.fl.us.

14 The information is shown on a sign in the
15 in-person location on the project website and in
16 the hearing notifications.

17 Next slide. The public hearing was
18 advertised in the Florida Administrative
19 Register on FDOT's Public Notices website, the
20 project website and in the local newspaper.

21 In addition, adjacent property owners,
22 interested individuals, elected and appointed
23 officials and government agencies were also
24 notified about this public meeting.

25 Next slide, please. Next slide.

1 Sorry, back to 33. Sorry about that.

2 The public hearing was advertised
3 consistent with the federal and state
4 requirements shown on the slide.

5 Next slide.

6 The environmental review, consultation and
7 other actions required by applicable federal
8 environmental laws for this project are being or
9 have been carried out by FDOT pursuant to
10 23 U.S.C. 327, a Memorandum of Understanding,
11 dated May 26, 2022, and executed by the Federal
12 Highway Administration and FDOT.

13 Next slide, please.

14 The next step is to incorporate your input
15 on this public hearing into our decision-making
16 process. After the comment period closes and
17 your input has been considered, a decision will
18 be made and the final PDE document will be
19 approved.

20 This project has and will continue to
21 comply with all applicable state and federal
22 rules and regulations.

23 This concludes the formal presentation.

24 Next we move into your comments.

25 So thank you for attending the public

1 hearing and providing your input. (Applause)

2 And now we'll turn it over to Joseph. You
3 need a glass of water.

4 MR. FONTANELLI: Thank you, everyone, for
5 your patience. We truly apologize for that
6 technical difficulty.

7 We will now enter the formal public comment
8 period for this hearing.

9 Anyone who wishes to make a verbal
10 statement regarding the project will now have
11 the opportunity to speak.

12 All questions and comments will become part
13 of the public hearing record, and we will
14 respond to all questions in writing after the
15 hearing.

16 If you've already filled out a speaker
17 request card, you may provide your statement on
18 the microphone when called upon. We'll have the
19 microphone and will run over to you if you raise
20 your hand.

21 The project team members will hand out any
22 information cards if you want one. If you want
23 to speak, please raise your hand. We'll get you
24 that card. I already have a couple in my hand.

25 You may also provide your statement

1 directly to the court reporter at any time.

2 To ensure all who wish to speak today are
3 able to, the speakers will have a maximum of
4 three minutes to make a statement, and we will
5 respond to all questions in writing after the
6 hearing.

7 Next slide, please.

8 All right. We will now call upon
9 participants who have requested to speak.

10 Like I said, I do have some cards in my
11 hand, and we'll collect those as they're being
12 filled out right now. We'll then have you come
13 to the microphone when your name is called.

14 Please state your name and address. If you
15 represent an organization, a municipality, or
16 other public body, please provide that
17 information as well.

18 Again, we ask you to limit your comments to
19 three minutes. The timer on the screen will
20 reflect your remaining time. Please remember
21 that all these questions will be responded to in
22 writing after the hearing.

23 So I'll go to the cards that I have
24 received and then we'll collect those that are
25 being filled out.

1 And please forgive me if I mess up your
2 name, but I'll do my best.

3 Ron Sellers?

4 (Court Reporter lost power.)

5 (Power restored to reporter.)

6 SPEAKER: (In progress) look at South
7 Florida. Look at Albuquerque, New Mexico.
8 There are several easy ways to implement these
9 methods. We need more police presence and
10 accountability held on 524. Attention must be
11 paid for these individuals that are supposedly
12 supposed to benefit from this. What about the
13 protected cross walks? What about physical
14 barriers, speeding calm ways, nearby crosswalks?
15 Paint does not stop cars, people. Not to
16 mention, I have not heard one thing about
17 environmental monitoring. Who is paying for
18 pond overflow? I question. Is it the general
19 contractors or is it the state? Who is making
20 these environmental reports and estimates? Who
21 are the percentage of people that are sick from
22 mosquitoes? Who are the ones that are out there
23 testing the water quality on the rotation?
24 Without long-term testing, there is no
25 accountability of when pond overflows or when

1 discharging pollutants run off into our wetlands
2 or our aquifers. No matter how pretty the
3 picture looks, the more erosion makes in our
4 state of Florida gives more long-term water
5 contamination. And two ponds inside of the
6 Florida Department of Transportation right on
7 the way, right on the right of way. It's
8 supposed to it's supposed to be beneficial.
9 It's going to be a mosquito breeding nightmare.
10 We'll just spray them though, right? That's
11 what we're going to do? Absolutely not. I
12 demand that this community safety and
13 environment be prioritized before one more
14 building is approved or one single new lane is
15 added. My children's lives in this land's
16 future is nonnegotiable.

17 MR. FONTANELLI: Thank you for your time.
18 Thank you.

19 Again, thank you for your comment. Any
20 questions we'll respond to you.

21 Ms. Wessner, Michelle Wessner, Wessner,
22 Ms. Wessner. Did I say that right? Okay.
23 There you go. Okay. No, that's okay.

24 MS. WESSNER: Thank you. I'm not going to
25 spend the whole time introducing myself. I'll

1 try to -- normally I do talking for a living.
2 So I'm going to try to keep this short. But
3 I've lived here my whole life for 37 years. My
4 family actually helped build Cox Road. Our
5 family's been here a long time. My house is the
6 first house on the north side of Cox Road. My
7 grandfather built it. My husband has rehabbed
8 it. And we live there. So we are very invested
9 in this community. We have a small farm. We
10 have horses, cows. My husband owns an
11 electrical contracting business. And I know
12 most of my neighbors. And my biggest concern
13 here with the new plan or the proposed
14 alternative is these roundabouts. Very
15 concerned. Especially with just the lifestyle
16 that we have, especially on the north side of
17 524. I myself have a 50-foot rig, a living
18 quarters horse trailer, my kids rodeo. Cocoa's
19 now home to latest rodeo queen. We've got to
20 get her, my daughter, she's the new Little
21 Wrangler Princess. We've got to get her to
22 those rodeos. How am I going to get around 524
23 with my big, long horse trailer? We've got
24 boats. Most of my neighbors have boats. They
25 either have an ATV trailer. They have a camper.

1 They might have a trailer for their business,
2 painting, lawns, etc. We just got a crosswalk in
3 at Cox Road and 524, and I can't get my living
4 quarters trailer into my own road. So I'm very
5 concerned about trying to get through these
6 roundabouts. We do a lot of traveling with our
7 large horse trailer. And it's a nightmare going
8 through these roundabouts, particularly, and
9 there's a lot of studies that most people don't
10 know how to use a roundabout. I think I've seen
11 some studies where there's 40 percent confusion
12 as to how to use one. And if you don't own a
13 trailer and you're on the road with me, you
14 don't know how wide my turn needs to be. We
15 have the Walmart Distribution Center here. So
16 we are going to be in this roundabout with semis
17 going there. You're going to be on the road
18 with me with a 50-foot rig. My wheels are way
19 back there. I'm going to try to watch out for
20 you, but I know a lot of people don't know how
21 much room I need. And just recently, we went to
22 Middletown, myself with the big rig, my mom with
23 the camper, and it was a nightmare. We had
24 trucks going through there, and it's difficult.
25 I know there can be aprons, that sort of thing.

1 I have precious cargo. I have children, kids'
2 horses. If you have horses, it's very hard to
3 find the right match. So this is precious cargo
4 to me. I'm also a member of our local horse
5 club here right off of 524 on Friday Road, Bit
6 and Spur. This is not just me. There's two
7 large horse stables on both sides of Cox Road.
8 We've got Iron Horse, Stephanie Rivers right
9 across from the Humane Society. This is
10 impactful. This is not just a small area. So
11 I'm very concerned about these roundabouts. The
12 studies I saw were performed in suburban areas,
13 not in such an outdoorsy trailer-toting
14 community as the people who travel on 524. So I
15 have a couple of questions that I would like
16 answered. Have any of these studies been done
17 in equestrian or boating communities? And what
18 were those results and follow-up results after
19 they were implemented? Were there any studies
20 that accommodated not just the daily traffic,
21 but what about the weekends and holiday traffic
22 when we start breaking our boats out and want to
23 go onto the east side to Publix to get our pub
24 sub to go boating, which is, I feel, like, vital
25 to anyone who boats here. And my last question,

1 sorry. Thank you for this time. Are these
2 studies and who's reviewing these really looked
3 at by people who are actually driving these
4 trailers and not just data? Those are my
5 questions that I would like an explanation for.

6 MR. FONTANELLI: Thank you.

7 MS. WESSNER: Thank you. And thank you for
8 listening twice.

9 MR. FONTANELLI: Mr. Fred Frazier.

10 Again, we have three minutes per speaker.

11 MR. FRAZIER: All right. Make no mistake.
12 These people were here making the presentation
13 only because the law says they have to. So
14 don't have any delusions that they're really
15 going to listen to anything that we input.
16 Period.

17 Let me start by saying two things, two
18 things. For over two years, we've listened to
19 the backup alarms from the construction of the
20 apartments on 524. 8:00 in the morning till
21 6:00 at night, six dang days a week. I implore
22 you to do something about the level of the
23 backup alarms of the construction equipment.
24 It's nonsense that they get away with this just
25 because it's from the state near the county or

1 the city. I mean, you can't do it. I have to
2 go outside with earplugs and noise-canceling
3 headphones to go outside my freaking house. Do
4 something about it. They don't have to be that
5 loud. It's over a mile away. I have to go in
6 my house, and I can still hear them.

7 The second thing is the roundabouts, just
8 like she said. Absolutely no one that I know of
9 for years now since they put in the one at
10 Wickham Road and Walmart and now the one at
11 Viera Boulevard. They're too damn small. I've
12 driven roundabouts in Boston many, many times.
13 They're big. They work. They work in light
14 traffic, and they work in heavy traffic. The
15 ones that are proposed here is absolutely -- the
16 ones that you're proposing putting in are way
17 too damn small. I talked to the gentleman out
18 there and he said, "Well, they meet the state
19 requirements and they have them other places."
20 That doesn't improve things. That doesn't make
21 them good enough. Absolutely nobody I know in
22 this whole area likes the roundabouts.
23 Everybody I know hates them. And you want to
24 put in two more, three more? They're too small.
25 Read my lips. Too small. Just because you're

1 stingy enough and you don't want to buy the
2 right amount of real estate, you don't want to
3 take the property, any property that pays taxes.
4 Roads don't pay taxes, so you don't want to take
5 that and put it into a road. Make them just
6 turn loose with a few cents. Buy the right
7 amount of real estate and do it right instead of
8 putting in something that everybody from here on
9 forever hate driving through. And the amount of
10 truck traffic that's on 524, it's way, way
11 heavier than on Wickham Road or Viera Boulevard.
12 Way heavier. Don't be so irresponsible. I
13 mean, do your job right.

14 MR. FONTANELLI: Thank you for your
15 comments, sir. Thank you.

16 All right. Our next speaker, Pat Scott.
17 Pat Scott.

18 MS. SCOTT: Is it on?

19 MR. FONTANELLI: It sounds like it, ma'am.

20 MS. SCOTT: Okay. Yes, I'm Pat Scott. I
21 live off of North Friday Road, and my late
22 husband and I were owner builders in 1986. I
23 have lived there all but four years of that
24 time. And it is beautiful. I love it rural. I
25 love the woods. I love the wildlife. And I am

1 the trash pickup lady out there for Keep Brevard
2 Beautiful. And my assistant is here sitting
3 back there.

4 I don't want to embarrass him by
5 introducing him. But we technically cleaned up
6 2.1 miles from 524 all the way north to James
7 Road. And LJ maintains three-quarters of it
8 now. But you know a lot of it was when we were
9 here when they were talking about the Flying J
10 Truck Stop and stuff and the Distribution Center
11 was that they -- you were talking about
12 roundabouts then. And normally, I would say I'm
13 not in favor of them, but I have news for you.
14 We hear a lot of tractor-trailer trucks because
15 they shortcut through State Road 524 from I-95.
16 And I am in favor of the roundabouts because
17 they beat traffic lights if you have to add
18 traffic lights to all of these places. I am
19 very much in favor of them. I wish you could
20 put a roundabout where I exit on 524, but we've
21 got the Flying J Truck Stop and the Distribution
22 Center. But it would definitely, when you widen
23 the road, it's going to attract a lot more
24 tractor trailer trucks. They're not going to
25 like roundabouts. And I say yay. So yeah. And

1 it would be nice to have some kind of sound
2 walls or sound barriers along the interstate.
3 But I had inquired about that many years ago,
4 and they said, well, we were rural residential,
5 so we didn't have enough population there, I
6 guess. But please, please be supportive. I
7 have seen a lot of accidents because I pick up
8 right around the corner of 524, and it is scary.
9 Thank you.

10 MR. FONTANELLI: Thank you. Thank you.
11 All right, my next speaker, Ms. Nancy
12 Maruwalter.

13 MS. MARUWALTER: Maruwalter.

14 MR. FONTANELLI: I apologize.

15 MS. MARUWALTER: I've been here over 30
16 years myself. It's changed a lot. I'm very
17 rural. I live up in Canaver Rose (ph). I'd
18 like it to stay that way. They put those
19 Adamson Creek things in there.

20 AUDIENCE MEMBER: We can't hear you.

21 MS. MARUWALTER: Oh, I'm sorry. I don't
22 want to talk too loud. But you put those
23 Adamson Creek houses and they're like really
24 close. You changed the dynamics. You're doing
25 it again. I don't agree with that woman.

1 Roundabouts will not slow people down. They're
2 going to go just as fast because nobody wants to
3 slow down. And they're going to -- like the
4 other gentlemen said -- they're not big enough.
5 And nobody's going to be watching these tractor
6 trailers. How are you going to see around them
7 when you're in it? That's all I have to say.

8 MR. FONTANELLI: Again, thank you. All
9 right. Ms. Katie Delaney.

10 MS. DELANEY: I just wanted to say that I'm
11 here to listen to everybody tonight. And if
12 anyone would like to share with me, I'll be here
13 tonight. And also, you can email my office at
14 D1.Commissioner@brevardfl.gov. And definitely
15 send your thoughts and feelings to FDOT as well
16 because that's how it gets into the record. But
17 if you want to share that with me as well, I can
18 help advocate on behalf of you guys as we
19 continue on with this project.

20 MR. FONTANELLI: Mr. Dave Rocque.

21 MR. ROCQUE: First of all, can everybody
22 hear me? Good.

23 I'd like to reiterate what was said
24 earlier, and that is, I'm disappointed, really,
25 that no Cocoa representative was here and made

1 available to answer questions. I understand
2 somebody is here, but not necessarily to answer
3 the questions. And I think they're difficult
4 questions. I don't think they really want to
5 answer them. Because in my opinion, Cocoa has a
6 lot to do with this. I worked for the City of
7 Cocoa for 10 years with the fire department, so
8 I have a little bit of idea how Cocoa operates.
9 But I'm really disappointed that nobody's here
10 from Cocoa because let's face facts. They
11 created part of this problem. Right now FDOT is
12 here to correct the problem that Cocoa was
13 instrumental in instigating. And by that, I
14 mean, we have, as you all know, four apartment
15 projects on 524, two of them complete, two of
16 them still waiting to be completed. And I don't
17 know what else is going on except there's going
18 to be more. So by the time this project
19 actually gets started, there's not going to be
20 any land available for any more increases. And
21 Cocoa is looking at -- and I understand this --
22 it's all about the money. They are looking at
23 what kind of taxes can we get from those
24 apartments and all. You know, that's what
25 they're really looking at. Did they really take

1 into consideration what's goes on on State Road
2 524 with the present situation with how much
3 traffic we have there right now? It's
4 unbelievable. What we have is Amazon. We have
5 FedEx. We have all the construction trucks
6 going in and out all day long. And really, let's
7 face it, you can only go as fast and 524 is the
8 slowest truck on the road.

9 MR. FONTANELLI: Again, thank you, sir, for
10 your comment. We'll respond to you in writing.

11 Ms. Kristen Lortie.

12 MS. LORTIE: Good evening. My name is
13 Kristen Lortie. I am a resident who lives in
14 this area. I live off of 524 and I-95. And
15 then I'm also -- I've gotten so interested in
16 citizen advocacy, like meetings like this and
17 making them relevant that I went to work for our
18 county commissioner, D1 County Commissioner, who
19 wishes to listen to you and to hear all of your
20 comments. So I'm concerned about the
21 roundabouts. I weighed in, actually, back in
22 2019 with the first round of public comments.
23 And now we don't even have options because
24 they've put the roundabouts onto the design. So
25 that means that this is really the only thing

1 that we're reacting to. But back a few years
2 ago, they actually had more options. And I feel
3 like this project needs more input and more of a
4 working group input, not just, let's come up to
5 the microphone and then not know where our
6 comments go. So a working group where people
7 that have all of these various different
8 comments, I want to hear what everyone has to
9 say. I think everyone wants to really hear each
10 other. This format doesn't allow that. Even
11 the idea that we don't get answers back now from
12 the answers that they do have, because of this
13 format, it's too constricting for us. So I just
14 want to put that on record. So I'm concerned
15 about the roundabouts. I'm concerned about the
16 divergent diamond. I don't see the need that I
17 have at my house to have that extra structure
18 there. I'm not sure that I'm against it, but
19 I'm not sure that I'm for it. And I'm not sure
20 that -- this, by the way, costs \$7 million in
21 design is what's estimated for this project. \$7
22 million. So I'm not supportive of that, and
23 still we're out there pestering the design
24 people, the Commissioner and I to try to figure
25 out why that costs so much. But regardless,

1 this is a very important project. And I feel it
2 needs more input. I feel that we're being
3 shorted. I am glad that they are having this
4 forum, though, to at least receive our comments,
5 get our comments on the record. And they have
6 committed to the Commissioner and myself that
7 they are going to come up and report on this.
8 But I still think we need more than that. So
9 for people who want to get engaged, the
10 Commissioner also has a substack. We've done a
11 whole substack article on this and promoted this
12 event. And that can be found by searching Katie
13 Delaney Substack. And you can find that on
14 Google. And then also the Commissioner has a
15 Facebook page. So let's get more involved.
16 Feel free to interact with us. Sign up for the
17 Substack. If there's enough interest in a
18 working group, write to our email address
19 because they're saying that they're willing to
20 listen to us. So then we have to collect. We
21 have to collect somewhere. So if we're going to
22 collect at the Commissioner's office, then let's
23 collect there. And we do have our next comment
24 is going to be the Councilwoman from Cocoa,
25 Lorraine Koss. So she did. We do have a

1 representative. We need more representatives
2 from Cocoa, but we do have one, so let's
3 appreciate the people who did show up. But
4 let's keep this conversation going rather than
5 just let this be the end of this and leave
6 frustrated. So find us where you can. You have
7 a Commissioner that cares, you know and let's
8 keep the input going.

9 MR. FONTANELLI: Thank you.

10 MS. LORTIE: Thank you very much.

11 MR. FONTANELLI: Thank you for your
12 comment. Lorraine Koss?

13 MS. KOSS: Good evening. And I guess I've
14 already been introduced, but I'm the
15 Councilwoman for District 4, which is not this
16 district. But nonetheless, everyone is impacted
17 by the project and by the congestion that we're
18 experiencing now. Pat Weeks is the
19 representative. She's great. She listens. I
20 strongly urge you to try to contact her. I know
21 she wasn't able to be here tonight. And I'm
22 sorry that we don't have more representation
23 because, obviously, there's a lot that needs to
24 be said and heard here. The development that's
25 been going on. I was a part of that. I did not

1 vote for all of it. In fact, in many cases, I
2 was the only one that didn't. But because I
3 have a lot of constituents that moved here
4 because they wanted to get away from all that.
5 So that's who I was representing. Nonetheless,
6 a council vote is a council vote. And we are,
7 being a low-income city, a lot of the council,
8 several of my colleagues felt pressure to
9 develop when this opportunity is here because so
10 for so many years, we didn't have development
11 opportunities. Now, whether we've done that
12 correctly?

13 AUDIENCE MEMBER: (Not using microphone.)

14 MS. KOSS: Yes, I agree. I agree. And
15 that's why I voted against several projects.

16 THE REPORTER: I cannot hear.

17 MS. KOSS: And apartment construction is
18 who's coming to us to develop. And some of them
19 are supposed to be. But they're more than my
20 mortgage payment. So just to say that thank you
21 for making your voices heard and continue to do
22 that. And I hope that we have a better project
23 going forward. But I will say this. I will
24 defend traffic circles because I have lived in
25 places with traffic circles, less fatalities,

1 smoother traffic flow. And that is just I've
2 experienced that. I know you've got a truck. I
3 know, but believe me.

4 MR. FONTANELLI: Ma'am, ma'am. Ma'am, this
5 is her time, ma'am.

6 MS. KOSS: And I do like the approach of
7 creating some pedestrian the pedestrian pathway
8 and bicycle pathway. I'm really glad to see
9 that. So, thank you.

10 MR. FONTANELLI: Thank you, Councilwoman.
11 Miss Ashley Kemp? Ashley Kemp?

12 MS. KEMP: Hello. My name is Ashley. I've
13 lived in this area my entire life. I grew up on
14 Friday Road. I now live in the ranches off of
15 Friday Road. My husband has a heavy equipment
16 business. We do a lot of trailers with heavy
17 equipment. Also, I have horses, and I, myself,
18 pull trucks with trailers. Now, which I agree
19 with the gentleman in the yellow that spoke
20 about how the city has caused the problem with
21 the traffic. I don't believe the roads were
22 zoned for that development. I would love to
23 know how that got passed. I believe somebody
24 paid the right person, and that's why we're
25 having this issue now. My concerns are the

1 existing citizens that were there. It was a
2 rural area and community. That's why we lived
3 here before we were sold out. The roads need to
4 be wider. My cousin spoke earlier. She has a
5 huge trailer. She's lived here her entire life.
6 She can't make these turns. You don't know this
7 about the roundabouts. You don't know this
8 until you pull a truck and trailer. It's not
9 even about me driving the truck. It's about
10 statistically, 40 percent of people do not know
11 how to use roundabouts. That's the statistics.
12 They don't know how to use them. So you're
13 telling me that a car flies by me when I'm
14 trying to pull my truck and trailer because they
15 don't know how my radius of turning. Unless you
16 drive them, you don't understand that. The
17 semis and -- ma'am, I think you spoke, and I
18 appreciate what you do on Friday Road. I see
19 you out there picking up the trash. We love
20 that. That's beautiful. The problem is the
21 semi traffic is not going to stop. We have the
22 Distribution Center that's there now. You're
23 still going to have the semis. You're still
24 going to have that. I love the idea of widening
25 the roads. Make the roads wide enough for the

1 semis. Make the roads wide enough for the
2 trucks and trailers. The roundabouts are not
3 going to work. They don't work unless you drive
4 it yourself like I do. I can pull a truck. I
5 can pull a trailer. I work for a transportation
6 company off of Cox Road, which is going to be
7 impacted. It doesn't work. It's not about the
8 people driving these things. It's about the
9 other people that don't know how to use them.
10 So when designing this, look at the rural area.
11 Look at the statistics. Look at the drivers who
12 are going to be using this. And it really does
13 go -- like Katie, you spoke -- it affects the
14 people that have already been here. It really
15 affects us. And you have to know -- and I hate
16 to say this, and I'm going to say it -- people
17 are not riding their bicycles and walking down
18 524. We don't need it. We don't. We don't
19 need that. What we need is wide lanes where
20 these trucks can pass safely, where we don't
21 have accidents, where these buses can pull in
22 off of 95, and they can make these turns without
23 causing accidents. Roundabouts don't work. I'm
24 sorry. They don't work.

25 MR. FONTANELLI: Thank you, ma'am. Thank

1 you. Ms. Dorothy Dickey? Ms. Dorothy?

2 MS. DICKEY: Hello? Yep, there it is. I'm
3 going to try and do this without crying, but I
4 can't make any promises about the roundabouts.
5 One lady mentioned less fatalities. There
6 should be no fatalities. It's hard for people
7 to understand. I am a statistic. My father was
8 killed in an automobile accident three months
9 before I was born. I never knew my father. And
10 I hope and pray that nobody else has to go
11 through anything. I mean, I grew up fine and
12 stuff, but you never know what anybody's going
13 through. And you never know with these
14 roundabouts, these big trucks, these trailers.
15 You just don't know what the person behind the
16 wheel is going through. You know, did they just
17 lose their job? Someone just found out they had
18 cancer. There's just so many things. And so
19 the roundabouts just are really concerning. And
20 thank you.

21 MR. FONTANELLI: Thank you for your
22 comment. Okay. Ron Seller. Is that correct?
23 Okay. Sir, you only have you only have the one
24 comment. Sir, you only have the one. You only
25 have that one comment, Sir. Yes, sir. One

1 three-minute comment.

2 AUDIENCE MEMBER: (Not using microphone.)

3 MR. FONTANELLI: Okay, thank you. Thank
4 you. Thank you, sir. Well, thank you, sir.
5 Sir, thank you.

6 Joseph Philhower? Joseph.

7 MR. PHILHOWER: All right. Probably one of
8 the younger people in the room. I bought off of
9 524. I live off of Westminster. My work truck
10 is outside. I actually do pest control. So
11 everybody's already mentioned roundabouts. I
12 work in Viera. I'm an account manager, an
13 assistant manager at this pest control place.
14 It's a local business. Mosquitoes are going to
15 come up from these ponds. Are they going to
16 have fountains completely sidestepping
17 everything else? We've already touched on
18 roundabouts do not work in areas that aren't
19 cities. So every day in Viera, there's a wreck.
20 My work truck actually got hit in Viera at a
21 roundabout. So I can clearly say that they
22 don't work. Maybe in an area that's a little
23 bit less congested, it's only going to get more
24 congested with 40,000 people in Cocoa,
25 unfortunately. Everybody's kind of already

1 mentioned everything else. I'm a
2 third-generation Brevard native, never moved
3 away. Rockledge High. To see where I bought my
4 first house growing into an area that's going to
5 cash me out and I won't be able to afford my
6 property taxes after stuff like this happens
7 years down the road. It's mind-boggling. I
8 don't have a kid yet. But when I do, I want
9 them to have the same thing I grew up with, not
10 just an overwhelming amount of highways and
11 whatnot. The median, to me, seems very wide
12 looking at this picture. I think that there
13 should be less of a median in the middle of the
14 road and then more of an easement on the side.
15 Cocoa is not in affluent area, as everybody
16 knows. Unfortunately, it's old money. Nothing
17 wrong with that. But when we have broke down
18 cars, if they're broke down in the roundabout,
19 who's going to tow it out? My car actually got
20 hit the same week my work truck got hit in
21 Cocoa. It was on US-1, but it took a day for my
22 tow company to come out. Don't have that
23 insurance company anymore. But there's a lot of
24 big trucks. They have flat tires. They aren't
25 going to be completely on the road at all times

1 operational. Everybody has nowhere to pull off
2 right now. I don't really think that's going to
3 help. If we have a 14-foot-wide paths for
4 pedestrians, the apartments are going to have
5 those. And it's not really something that I see
6 even my kids running down and playing on 524 in
7 the future. So I don't know. That's all I got.

8 MR. FONTANELLI: Thank you, sir. Thank
9 you.

10 That's all the speaker cards I have.

11 Okay. On behalf of the Florida Department
12 of Transportation, thank you for attending this
13 public hearing and providing your input on this
14 project. If you have comments or questions
15 after the hearing, please submit them by
16 February 2nd, 2026.

17 It is now 7:17 p.m. and I hereby close,
18 officially close, the public hearing for State
19 Road 524 PD&E study. Have a good safe evening.

20 (The public hearing concluded at 7:17 p.m.)
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23
24
25

C E R T I F I C A T E

STATE OF FLORIDA)

COUNTY OF ORANGE)

I, CYNTHIA R. GREEN, Court Reporter,
certify that I was authorized to report and
transcribe the aforementioned Public Hearing and
that the transcript is a true and complete record of
the recording.

I further certify that I am not a relative,
employee, attorney or counsel of any of the parties,
nor am I financially interested in the outcome of
the foregoing action.

DATED this 6th day of February, 2026.

Cindy R. Green

CYNTHIA R. GREEN, Court Reporter
Notary Public, State of Florida
(electronic signature)

Commission Expiration: 07/02/28
Commission No.: HH 567524