

Florida Department of Transportation



SR 524 PD&E Study

SR 524 from Friday Road to Industry Road
Brevard County, FL

Conceptual Stage Relocation Plan

FDOT Office
District Five

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The environmental review, consultation, and other actions required by applicable Federal environmental laws for this project are being, or have been, carried out by FDOT pursuant to 23 U.S.C. & 327 and Memorandum of Understanding dated May 26, 2022 and executed by Federal Highway Administration and FDOT.

Draft

EXECUTIVE SUMMARY

The Florida Department of Transportation (FDOT) District Five is conducting a Project Development and Environment (PD&E) Study to evaluate alternative improvements along State Road (SR) 524 from Friday Road (south) to Industry Road in Brevard County, a total distance of approximately 3.15 miles. This study includes traffic analysis, engineering analysis and environmental impacts evaluation, pond siting, agency coordination, and public involvement including meetings and a public hearing.

Additional right of way (ROW) will be required for the roadway improvements and stormwater management facilities included with the Preferred Alternative. The Preferred Alternative is anticipated to require one business relocation, and no residential relocations. The one business relocation is Tiger Mart, a Convenience store and gas station. Replacement vacant commercial sites are available for purchase and lease in the area around the project.

The Florida Department of Transportation will carry out a Right of Way and Relocation Assistance Program in accordance with *Florida Statute 421.55*, Relocation of displaced persons, and the *Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970* (Public Law 91-646 as amended by Public Law 100-17).

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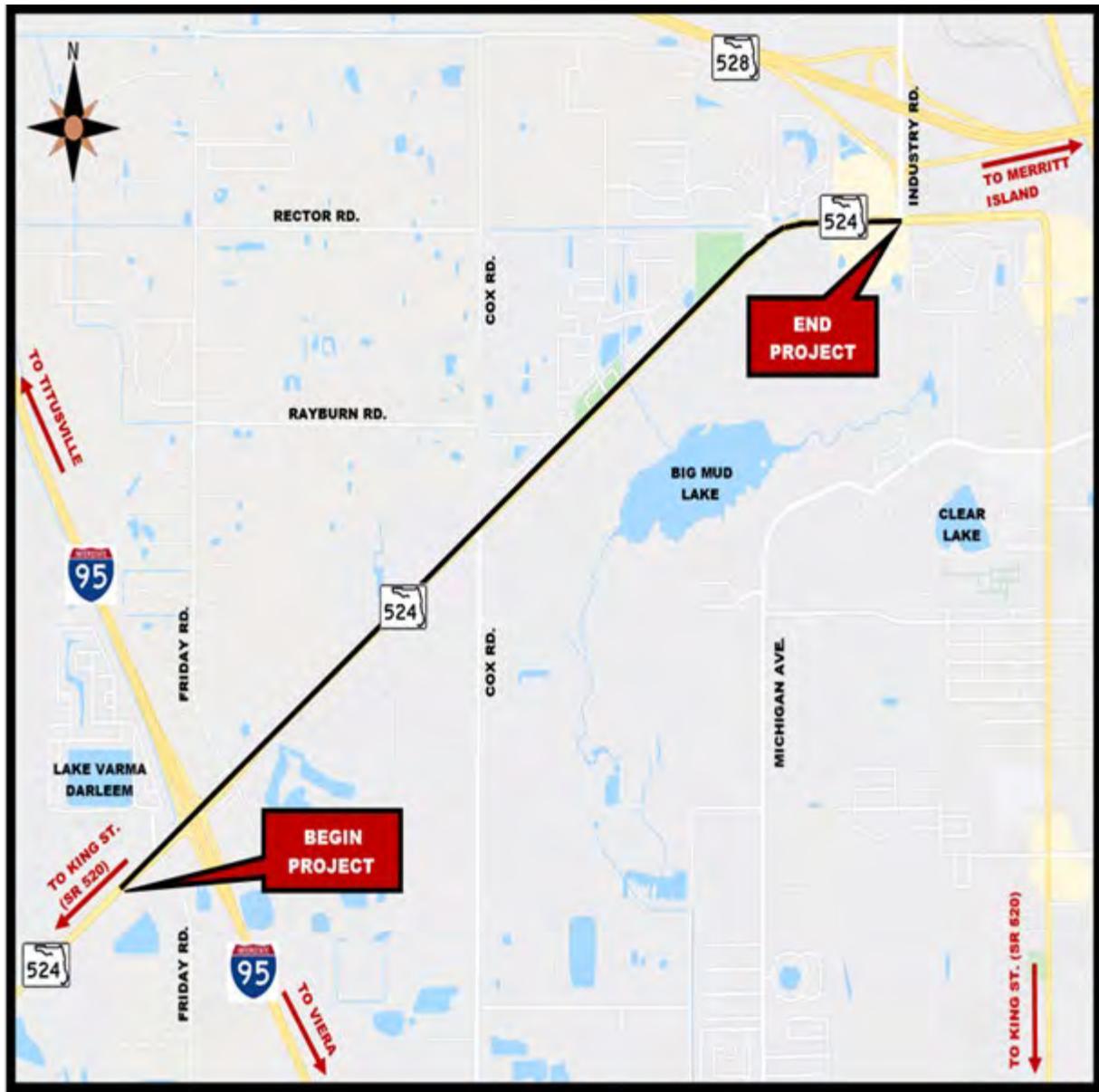
1. INTRODUCTION

1.1 PD&E STUDY PURPOSE

This project aims to increase capacity by widening State Road SR 524 between Friday Road (South) and Industry Road (see **Figure 1-1**) while improving safety along the corridor for all users. This project is part of an effort to improve the current conditions, so they are projected to meet future standards of Level of Service (LOS), safety, traffic flow, and improve accessibility to large trucks and pedestrians and cyclists alike.

The study's objective is to evaluate roadway, intersection, and interchange alternatives associated with the widening of SR 524. In addition, the study will analyze and assess proposed impacts on the social, economic, cultural, natural, and physical environment to develop the location and design concept of the project in accordance with Florida Department of Transportation (FDOT) policy, procedures, and requirements.

Figure 1-1 Project Location Map



1.2 PROJECT DESCRIPTION

The FDOT is conducting a Project Development and Environment (PD&E) Study for an approximately 3.15-mile portion of SR 524 between Friday Road (South) and Industry Road in Brevard County. Within the project limits, SR 524 exists as a two-lane urban minor arterial comprised of one 12-foot lane in each direction with 10-foot shoulders (4-foot paved). Intermittent sidewalks are located along the north side of SR 524 between Cox Road and Industry Road and along the south side from the CVS signalized intersection to Industry Road, while the existing paved shoulders serve as undesignated bike lanes. Stormwater flows off the roadway into roadside ditches.

1.3 EXISTING CONDITIONS

The SR 524 corridor is primarily an east-west corridor through Brevard County that has direct access to the high-speed, limited-access interstate highway I-95 and indirect access to the high-speed limited access roadway of SR 528. The study's area of influence (AOI) extends from SR 520 to the west and SR 501 (Clearlake Road) to the east and includes the I-95 interchange and the Industry Road / SR 528 Interchange. The surrounding roadway network associated with the SR 524 corridor includes:

Interstate 95 (I-95)

Interstate 95 (I-95) is part of Florida's Strategic Intermodal System (SIS) and is functionally classified as an urban principal arterial interstate. Within the AOI, I-95 is a six-lane median divided limited access facility with three 12-foot travel lanes with paved inside and outside shoulders in each direction from south of SR 520 to the north of SR 528. The posted speed limit is 70 miles per hour (mph) within the study area.

SR 528

Also known as the Beachline East Expressway, SR 528 is an east-west corridor located just north of the SR 524 project study area. SR 528 is part of Florida's SIS, is functionally classified as an urban other expressway, and falls under the jurisdiction of FDOT District 5. SR 528 exists as a four-lane divided highway with paved inside and outside shoulders in each direction. The SR 528 / Industry Road interchange lies just north of the SR 524 corridor with entrance / exit ramps that tie directly to the intersection at Industry Road and SR 524. The posted speed limit of SR 528 is 55-mph within the study area.

SR 501 (Clearlake Road)

Located at the eastern terminus of the project is SR 501 (Clearlake Road). Existing as a two-lane undivided roadway, SR 501 is under design, as of October 2024, to add capacity to a four-lane divided urban roadway that includes sidewalks and bike lanes for multimodal users. This project is also in the PD&E stage where analysis began in 2015.

The SR 524 corridor contains an interchange with I-95 between Friday Road (South) and Friday Road (north) and eight signalized intersections. Improvements to the existing I-95 diamond interchange and the signalized intersections are included in this PD&E Study.

SR 524 is a Two-Lane Urban Minor Arterial. It is part of the Strategic Intermodal System (SIS). SR 524 is designated as an evacuation route by the Florida Division of Emergency Management. I-95 is a six-lane divided limited-access freeway designated as an evacuation route. The approved context classifications on SR 524 are:

Segment 1: Friday Road (south) to Friday Road (north)	C3C (Suburban Commercial)
Segment 2: Friday Road (north) to Cox Road	C3R (Suburban Residential)
Segment 3: Cox Road to London Boulevard	C3R (Suburban Residential)

The existing typical sections are shown in **Figure 1-2** through **Figure 1-4**.

Figure 1-2 Existing SR 524 Typical Sections

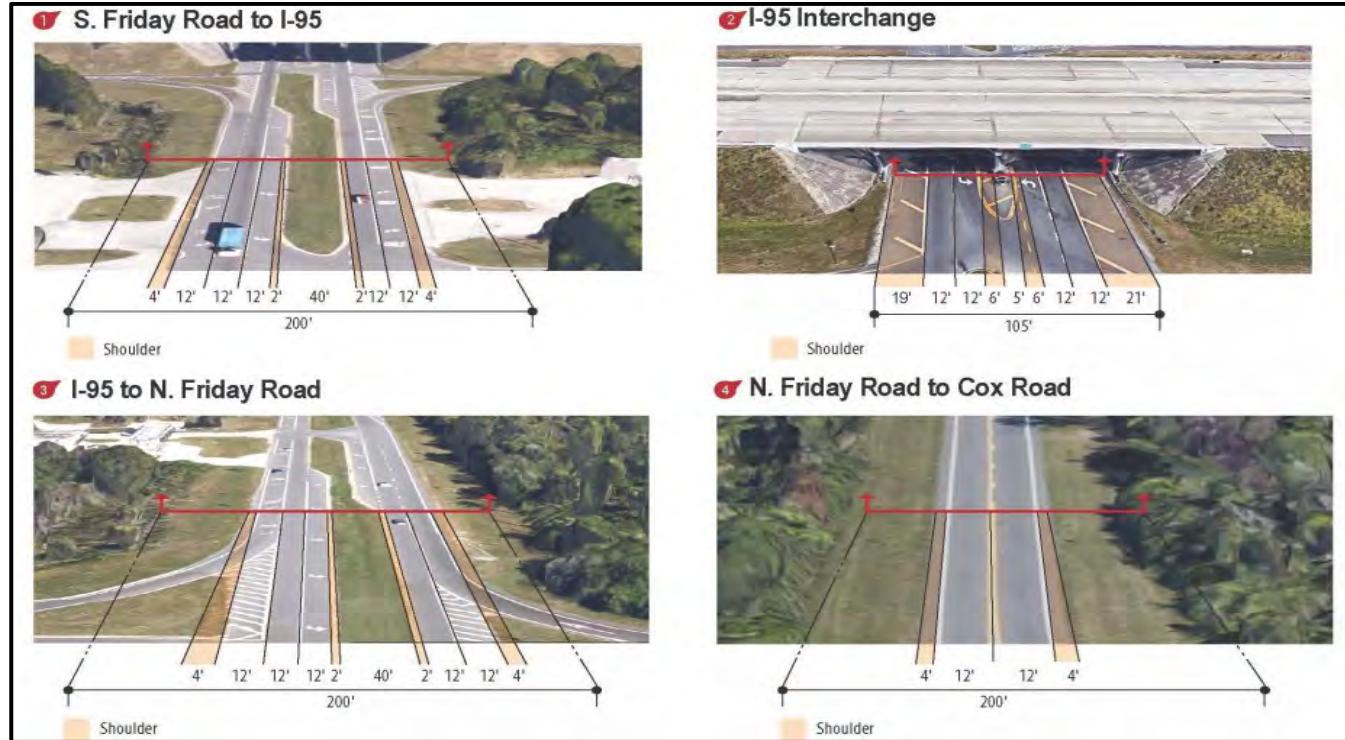


Figure 1-3 Existing SR 524 Typical Sections

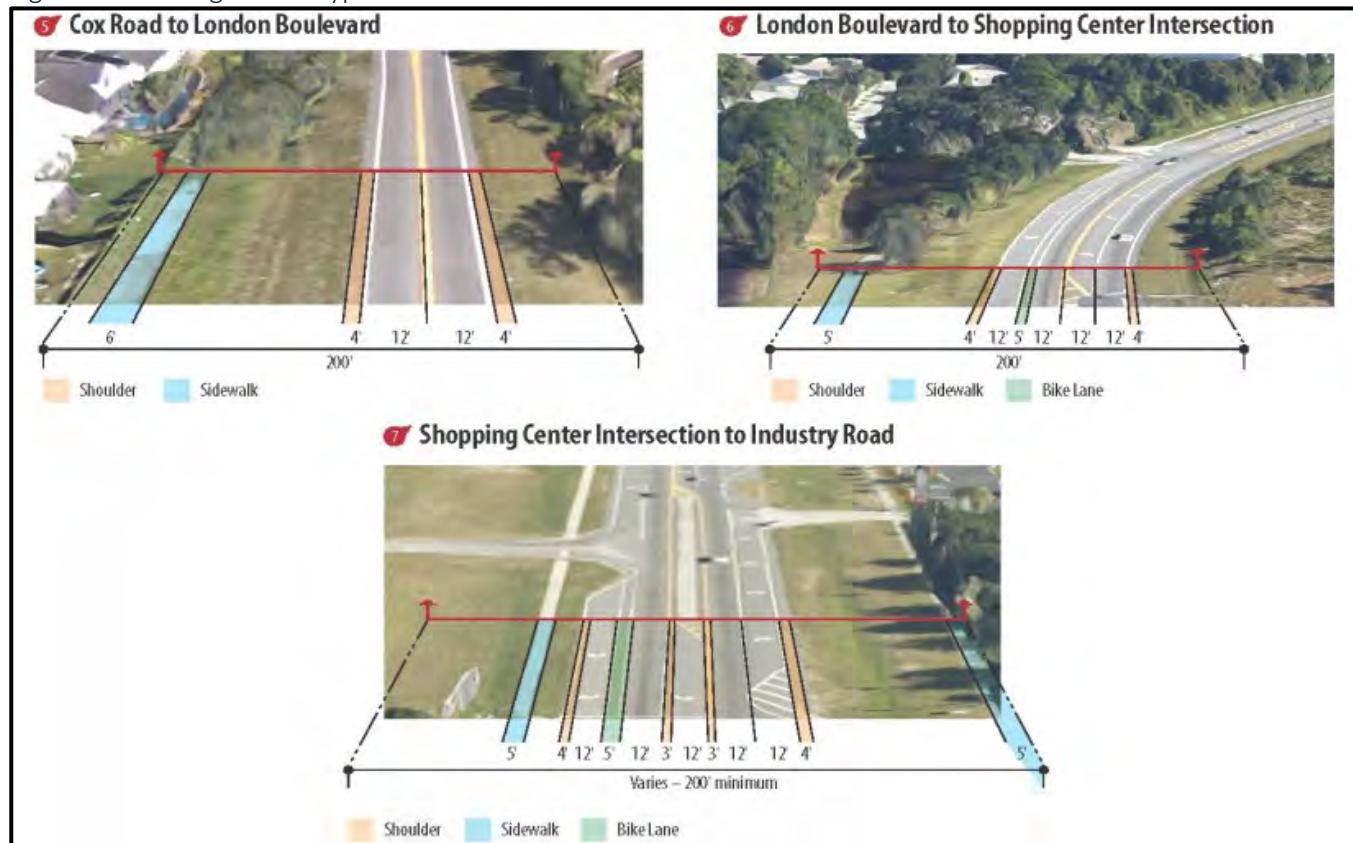
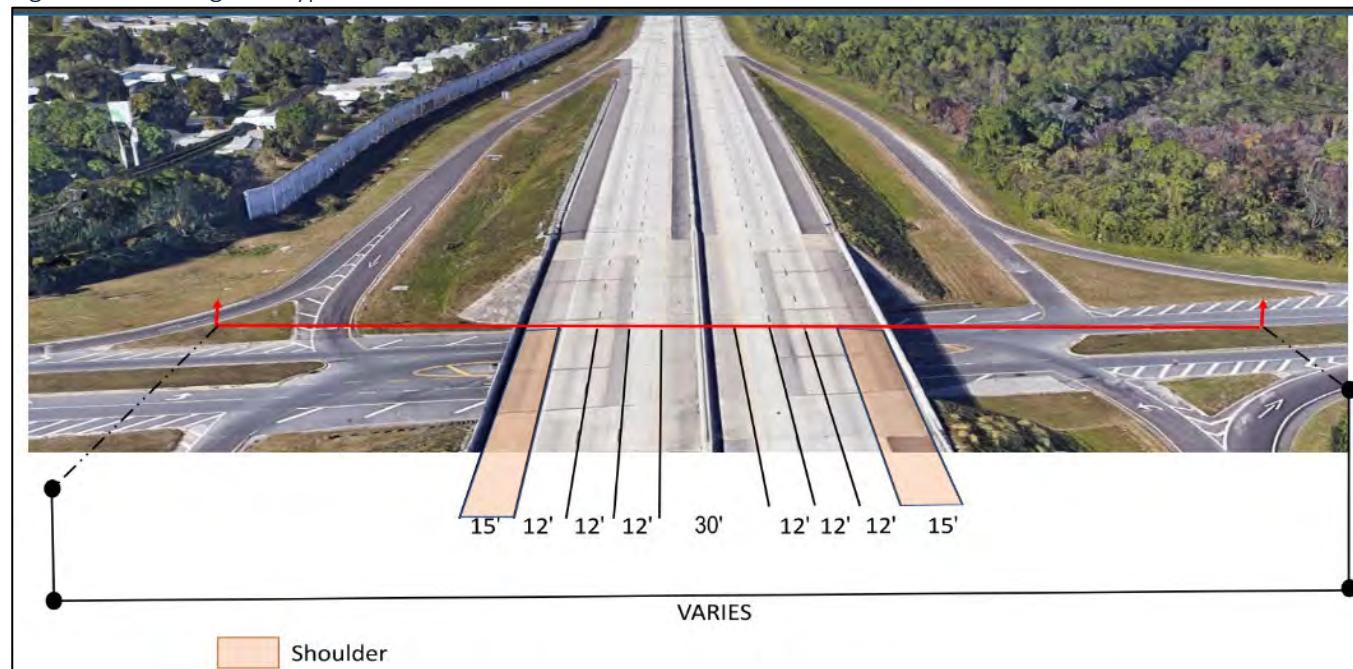


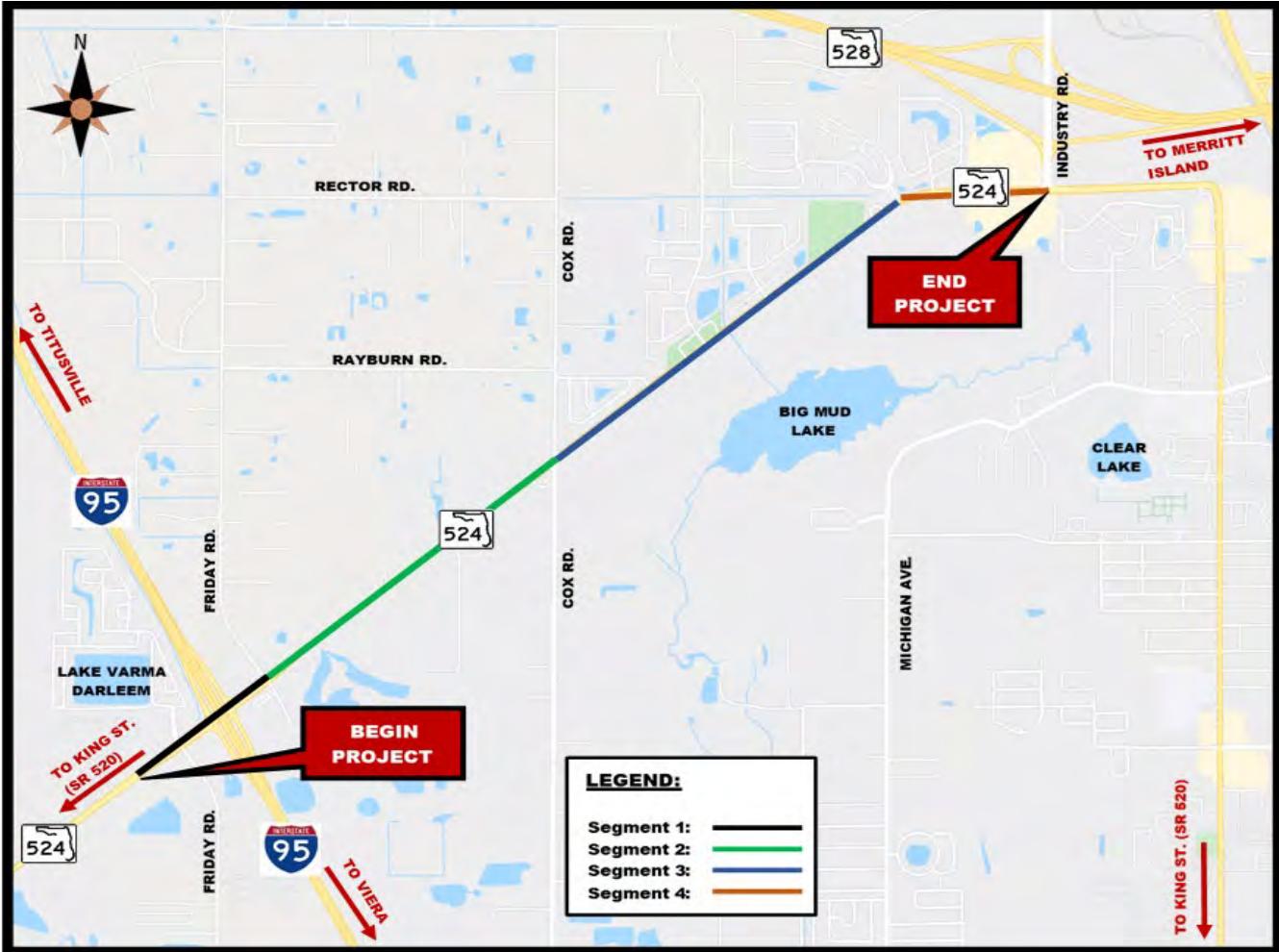
Figure 1-4 Existing I-95 Typical Section



1.4 PROPOSED IMPROVEMENTS

The limits of the study are shown below in **Figure 1-5** and shown on the Concept Plans in **Appendix A**.

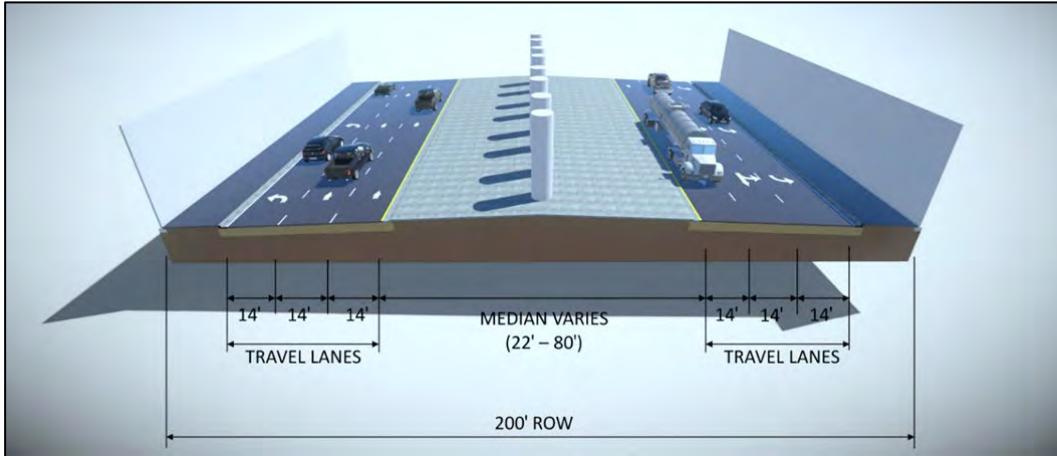
Figure 1-5 Project Segment Map



1.4.1 Segment 1 S.R. 524 from South Friday Road to North Friday Road

Segment 1 will be a four-lane divided section that runs between South and North Friday Roads with a DDI at I-95. The typical section outside the Diverging Diamond Interchange (DDI) has 12-foot travel lanes (modified from the 11-foot lanes shown in FDM 306.5), Type F outside curb and gutter, Type E inside curb and gutter, 14-foot shared-use paths on either side of the corridor 4-foot from the ROW, and a varying median (48-foot south, 53-foot north). The travel lanes widen to 14-foot within the Diverging DDI limits and include an additional 14-foot left-turn lane, see **Figure 1-6**.

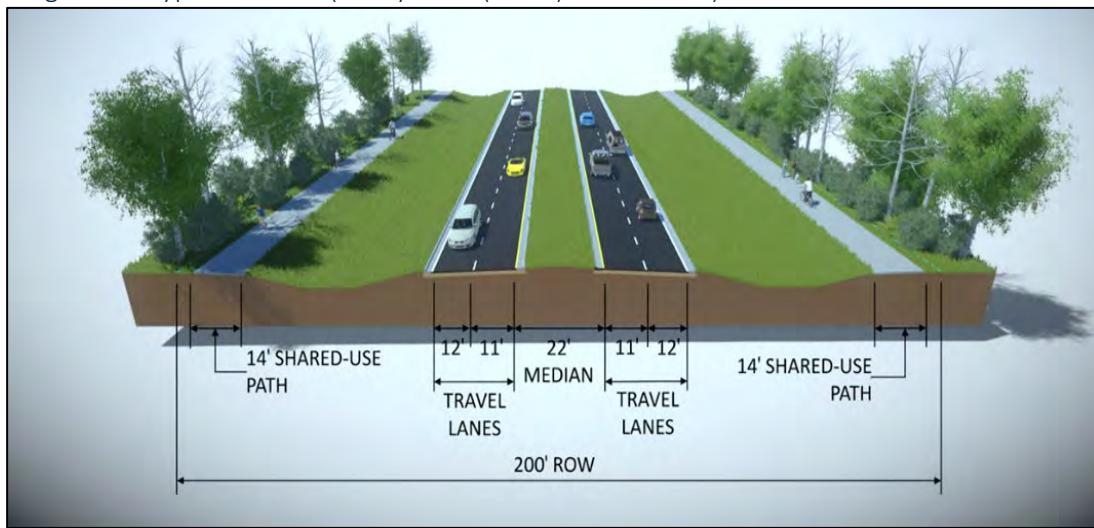
Figure 1-6 Segment 1 Typical Section (Friday Road (South) to Friday Road (north))



1.4.2 Segment 2 Typical Section (Friday Road (north) to Cox Road)

Segment 2 will be a four-lane divided section that runs from Friday Road (north) to Cox Road. This section has Type F outside curb and gutter, 12-foot outside lanes (modified from the 11-foot lanes shown in FDM 306.5), 11-foot inside lanes, Type E inside curb and gutter, and a 22-foot median (17.5-foot sodded). 14-foot shared-use paths are on each side of the corridor 4-foot from the existing ROW. 5-foot-wide drainage swales with 1:4 front and back slopes will be placed between the shared-use path and the outside curb and gutter to capture off-site drainage as required, see **Figure 1-7**.

Figure 1-7 Segment 2 Typical Section (Friday Road (north) to Cox Road)



1.4.3 Segment 3 Typical Section (Cox Road to London Boulevard)

Segment 3 will be a four-lane divided section that runs from Cox Road to London Boulevard. This section has Type F outside curb and gutter, 11-foot travel lanes, Type E inside curb and gutter, and a 22-foot median (17.5-foot sodded). The median widens to a maximum of 60-foot in the chicane section where the road diverges. 14-foot shared-use paths are on each side of the corridor 4-foot from the existing

ROW. 5-foot-wide drainage swales with 1:4 front and back slopes will be placed between the shared-use path and the outside curb and gutter to capture off-site drainage as required, see **Figure 1-8**.

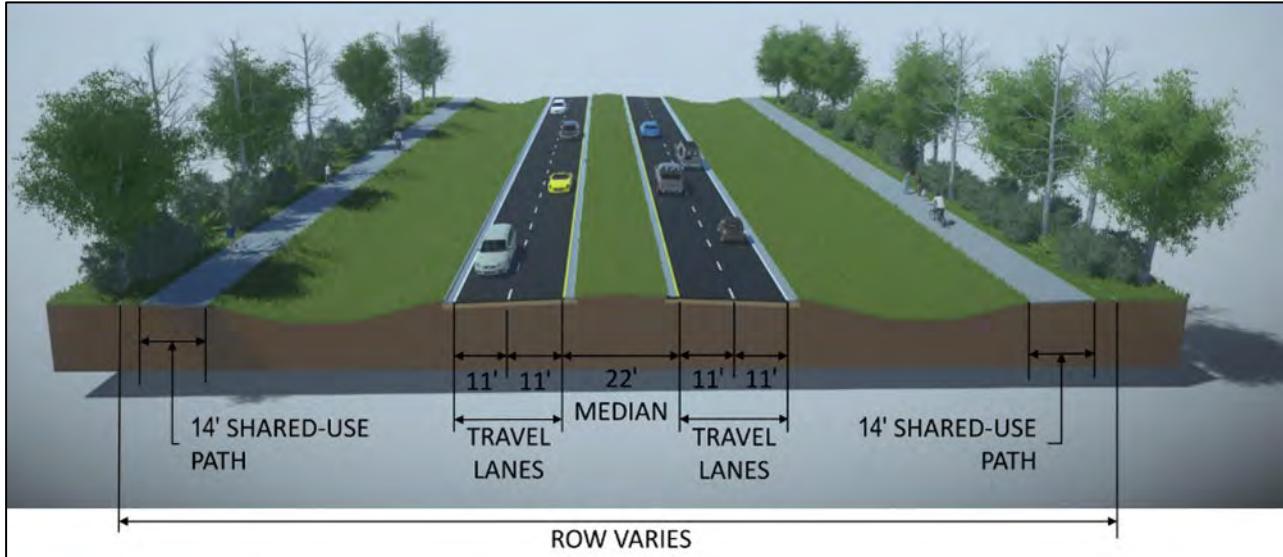
Figure 1-8 Segment 3 Typical Section (Cox Road to London Boulevard)



1.4.4 Segment 4 Typical Section (London Boulevard to Industry Road)

Segment 4 will be a four-lane divided section that runs from London Boulevard to Industry Road. This section has Type F outside curb and gutter, 11-foot travel lanes, Type E inside curb and gutter, and a 22-foot median (17.5-foot sodded). 14-foot shared-use paths are on each side of the corridor. The existing ROW widens on the north side, but the centerline will be at the same offset from the southern ROW as in segments 2 and 3 (50-foot). 5-foot-wide drainage swales with 1:4 front and back slopes will be placed between the shared-use path and the outside curb and gutter except where the shared-use path comes in closer to the road to tie back into the existing curb ramp configuration at Industry Road, see **Figure 1-9**.

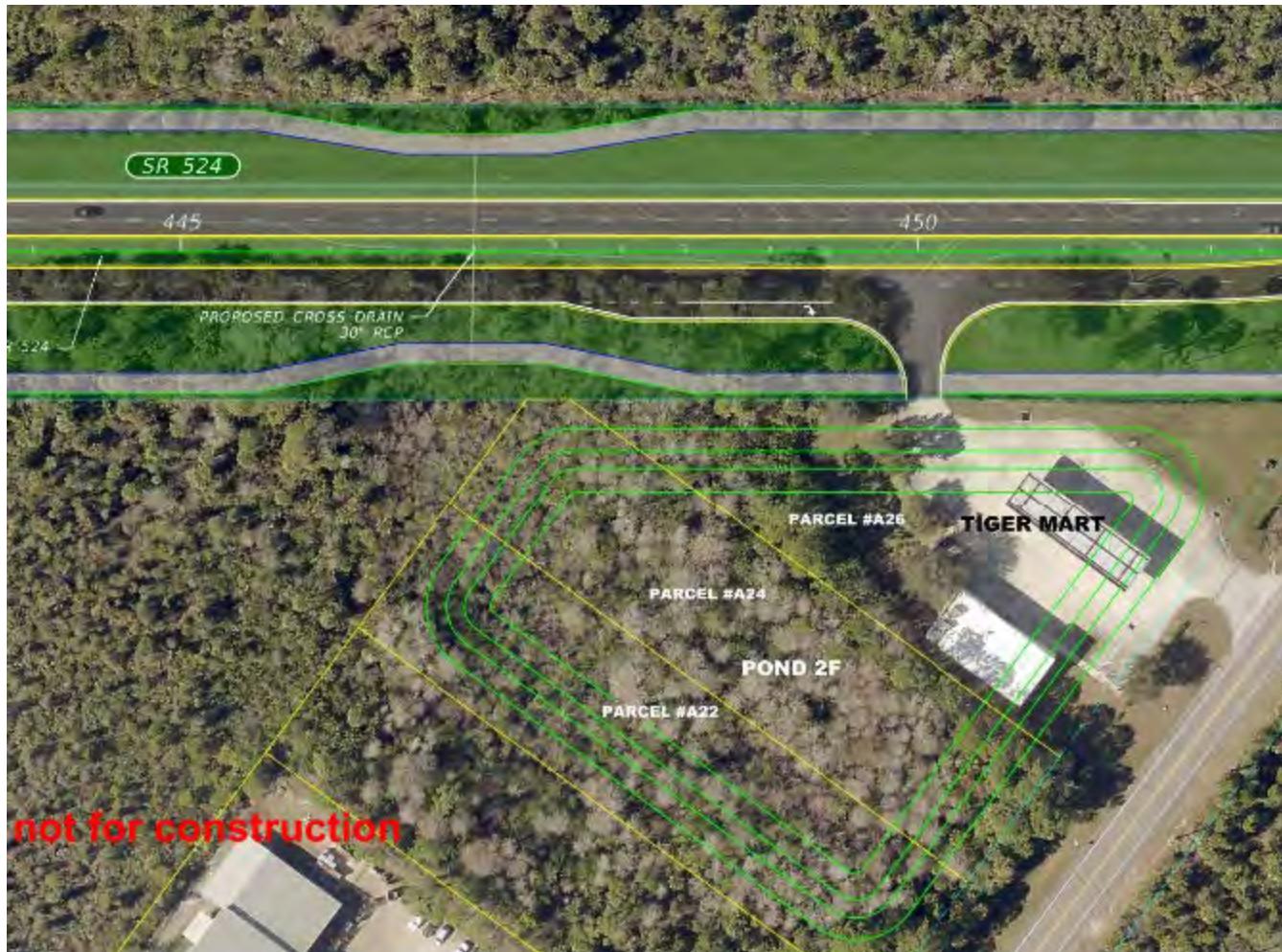
Figure 1-9 Segment 4 Typical Section (London Boulevard to Industry Road)



1.4.5 Pond Alternative 2F

This pond site is west of Cox Road, south of SR 524, and includes three adjacent properties as shown in **Figure 1-10**. Two of these sites are currently undeveloped parcels, each measuring 1.06 acres in size. The third site is a closed gas station, measuring 1.62 acres. This site was chosen after the Pond Site Alternative meeting held with the FDOT. The two vacant sites consist of wetlands. The potential for listed species habitat is low because no nesting, roosting, or foraging habitat was found at the site. The site is not located within or near the floodplain. The vacant sites were not checked for contamination risk as it was added later in the study. The gas station was found to be a medium rated risk site and recommended for further assessment.

Figure 1-10 Pond 2F Location



1.5 INTERSECTION IMPROVEMENTS

As part of the PD&E Study, different intersection concepts for Cox Road, London Boulevard, and Industry Road were evaluated. The configurations of the South/North Friday Road, Walmart Distribution Center, and Cirrus Drive intersections will also be updated to accommodate the proposed improvements. The final concepts selected for each intersection are shown on the Concept Plans in **Appendix A**.

1.6 PURPOSE OF THE CONCEPTUAL STAGE RELOCATION PLAN

This Conceptual Stage Relocation Plan (CSRP) is submitted in accordance with *Florida Statutes 339.09(2); 339.09(3); and 421.55; 49 CFR Part 24; and 23 CFR Part 710* to comply with the *Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970* (Public Law 91-646) as amended. The purpose of the CSRP is to evaluate the number and type of relocations resulting from the proposed project and to identify any socio-economic impacts to the surrounding neighborhoods that might occur as a result of implementing the Preferred Alternative. The acquisition and relocation program will be conducted in accordance with the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended. Relocation advisory services and resources are available to all residential and business relocatees without discrimination.

2. RELOCATION

2.1 RELOCATION OVERVIEW

The No-Build Alternative and the Transportation Systems Management and Operations (TSM&O) Alternative would not result in any relocations; however, this alternative would not address the needs of the proposed project. The Preferred Alternative would result in one business relocation and no residential relocations.

In order to minimize the unavoidable effects of right-of-way acquisition and displacement of residents, FDOT will carry out a Right-of-Way and Relocation Assistance Program in accordance with *Florida Statutes 421.55 (Relocation and Displaced Persons)*, and the *Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970 (Public Law 91-646 as amended by Public Law 100-17)*.

3. STUDY AREA

3.1 STUDY AREA CHARACTERISTICS

Figure 3-1 Census Tract Map



The US Census data for 2022 for Brevard County, Cocoa-Rockledge CCD and the affected census tract are found in the following tables:

Table 3-1 Estimated Percentages of Minority, Racial, National Origin, or Ethnic Households

Race	Brevard County	Cocoa-Rockledge CCD, Florida	Census Tract 625
Caucasian	84.3%	83.3%	62.8%
Black	12.3%	14.4%	33.3%
Other	11.8%	9.5%	8%
Hispanic*	11.4%	9.9%	9%

*Hispanics are not considered a race, and as an ethnic group are also included in the other categories.
Source: Census.Gov DP05 ACS Demographics and Housing Estimates: 2022

Table 3-2 Estimated Income Ranges (in USD)

HH Income	Brevard County	Cocoa-Rockledge CCD, Florida	Census Tract 625
< \$25,000 Per Year	17.0%	17.0%	31.0%
\$25,000 -\$49,999	22.5%	21.0%	32.1%
\$50,000 -\$74,999	18.7%	18.5%	17.6%
\$75,000 -\$99,999	12.8%	13.3%	8.1%
≥ \$100,000	29.0%	30.1%	11.1%
Median HH Income	\$63,632	\$65,632	\$33,833

Source: Census.Gov DP03 Selected Economic Characteristics 2022 American Community Survey

Table 3-3 Estimated Tenure (or age) of Structures

Housing Stock	Brevard County	Cocoa-Rockledge CCD, Florida	Census Tract 625
1969 or Earlier	21.3%	20.7%	34.5%
1970-1989	36.4%	29.1%	27.1%
1990-1999	15.7%	18.0%	13.3%
2000 and Newer	26.6%	32.1%	24.9%

Source: Census.Gov DP04 Selected Housing Characteristics American Community Survey 5-year Estimates 2022

Table 3-4 Estimated Percentage of Elderly Households in Relationship to the Total Households

Age	Brevard County	Cocoa-Rockledge CCD, Florida	Census Tract 625
Under 25 Yrs.	25.3%	24.3%	19.8%
25-44 Yrs.	22.2%	24.5%	24.9%
45-64 Yrs.	28.4%	29.2%	28.5%
65 Yrs. Or Older	24.0%	22.2%	26.2%
Median Age	47	45.7	49.6

Source: Census.Gov DP05 ACS Demographic and Housing Estimates 2022 American Community Survey 5-year Estimates

Table 3-5 Estimated Percentage of Households Containing Five or More Family Members

Criteria	Brevard County	Cocoa-Rockledge CCD, Florida	Census Tract 625
No. of Households	246,650	51,785	1,773
Average HH Size	2.45	2.45	2.83
% HH ≥ 5	7.00%	6.00%	6.00%

Source: Census.Gov B11016 Household Type by Household Size and S1101 Households and Families 2022

Table 3-6 Estimated Disabled Residential Occupants for Whom Special Assistance Services May Be Necessary

Criteria	Brevard County	Cocoa-Rockledge CCD, Florida	Census Tract 625
% Population Disabled	16.3%	16.2%	23.3%

Source: Census.Gov S1810 Disability Characteristics 2022 American Community Survey 5-Year Estimates

Table 3-7 Estimated Occupancy Status – Owner/Tenant

Criteria	Brevard County	Cocoa-Rockledge CCD, Florida	Census Tract 625
No. of Housing Units	290,314	59,865	2,251
% Occupied	85.0%	86.5%	78.8%
% Owner Occupied	76.7%	77.1%	63.3%
% Renter Occupied	23.3%	22.9%	36.7%

Source: Census.Gov DP04 Selected Housing Characteristics: 2022

4. RESIDENTIAL OVERVIEW

4.1 POTENTIAL RESIDENTIAL IMPACTS

No residential relocations have been identified.

5. BUSINESS OVERVIEW

5.1 POTENTIAL BUSINESS IMPACTS

One commercial business, Tiger Mart Amoco Gas Station, is proposed for relocation under the Preferred Alternative for the construction of Pond Site 2F. Information regarding the property is presented in **Table 5-1**, photographs are provided in **Figures 5-1** below. Property cards from the Brevard County Property Appraiser website for the impacted parcel are provided in **Appendix B**.

Table 5-1 Potential Business Impacts

Facility Name	Parcel Number	Address	Business Type	Year of Construction	Building Square Footage	Hazardous Waste Facility ID	Contamination Risk Rating
Tiger Mart	24-35-23-01-*1	4301 HIGHWAY 524 COCOA FL 32926	Convenience store/gas station	2002	3,200sf	9805290	Medium

Figure 5-1 Tiger Mart Amoco Gas Station



6. AVAILABLE BUSINESS SITES WHERE AFFECTED BUSINESSES MAY RELOCATE

Table 6-1 Commercial Properties for Sale

Address	Use	Year Built	Building Area	List Price	Comments
1502 W King St, Cocoa, FL 32926	Gas Station	1959	2476 SF	\$775,000	Former Gas Station

Source: Loopnet.com, Date: 10/4/2024

<https://www.loopnet.com/search/retail-properties/for-sale/?sk=13762628238d1209c0e5642740c5d1e4&bb=9wz7o3jy4G-w5R&loc=1502%2520West%2520King%2520Street%252C%2520Cocoa%252C%2520FL%252C%2520USA&e=u>

Table 6-2 Vacant Land for Sale

Address	Use	Lot Area	List Price
SR 524 and London Blvd. Cocoa, FL 32926	Vacant Land	1.62 acres	\$215,000
		1.28 acres	\$185,000
		1.26 acres	\$145,000
		1.01 acres	\$125,000
		1.45 acres	\$165,000
		1.98 acres	\$250,000
5275 Highway 520, Cocoa, FL 32926	Vacant Land	1.14 acres	\$399,000
W King St, Cocoa, FL 32926	Vacant Land	0.68 acres	\$350,000
HWY 520, Cocoa, FL	Vacant Land	2.76 acres	\$550,000

Source: Loopnet.com Date: 10/4/2024

<https://www.loopnet.com/search/land/for-sale/?sk=6850c8e90384499ee91d5ac05c4cadb0&bb=0xw-95qj5GrioooR>

7. LIKELIHOOD OF SUCH RELOCATION AND IMPACTS ON THE REMAINING BUSINESS COMMUNITY

Based on the selected alternative, it is estimated that a maximum of one business will potentially be displaced. It appears that there will be no impact on the remaining business community as a result of this displacement. The remaining business community will still be able to operate and do not appear to be dependent on the businesses that will be displaced.

8. PERSONAL PROPERTY ONLY RELOCATIONS

There are no personal property only moves noted on this project.

9. ON PREMISE SIGNS

On premise signs are handled during the appraisal and acquisition phase. The owner will be compensated for the value of the signs through the appraisal process.

10. POTENTIAL CONTAMINATION CONCERNS

The *Contamination Screening Evaluation Report* for this project has identified the 15 sites that have some risk of contamination impacts to this project. There are 5 Low, 4 Medium and 3 High Risk ratings. The subject site has a Medium risk for the Diesel Fuel and Unleaded Gasoline tanks.

11. PUBLICLY OWNED LANDS

When lands, buildings or other improvements are needed for transportation purposes, but are held by a governmental entity and utilized for public purposes other than transportation, the acquiring body may compensate the entity for such properties by providing functionally equivalent replacement facilities. Based on this study, there are no public facilities requiring functional replacement.

12. DISCUSSION RESULTS OF CONTACTS WITH LOCAL GOVERNMENTS, ORGANIZATIONS, GROUPS AND INDIVIDUALS REGARDING RESIDENTIAL AND BUSINESS RELOCATION IMPACTS

The FDOT conducted extensive public outreach for this project, including coordination with representatives from Brevard County. District invited representatives from the County to public meetings and provided one-on-one briefings for these representatives.

Several government agencies were contacted to inquire about specific resources available to the businesses impacted by the acquisition.

13. ACQUISITION AND RELOCATION ASSISTANCE PROGRAM

In order to minimize the unavoidable effects of right-of-way acquisition and displacement of people, the FDOT will carry out a Right-of-Way and Relocation Program in accordance with Florida Statute 339.09 and the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970 (Public Law 91-646 as amended by Public Law 100-17).

The FDOT provides advance notification of impending right-of-way acquisition. Before acquiring right-of-way, all properties are appraised to determine the market value. Owners of property to be acquired will be offered and paid fair market value for their property rights.

To ensure that relocatees receive adequate time to relocate, the FDOT hereby assures that they will not be required to move from the subject property before at least ninety (90) days have elapsed from the date of receipt of our 90 Day Letter of Assurance. This assurance applies to individuals, families, businesses, or farm operations; or personal property they may own. Further, the relocatee will be given a written notice which will specify the actual date by which the property must be vacated and surrendered to the FDOT. They will receive this latter notice at least thirty (30) days prior to the date specified and no occupant of a residential property will be required to move until decent, safe, and sanitary replacement housing is made available. "Made available" means that the affected person has either by himself obtained and has the right of possession of replacement housing, or the FDOT has offered the relocatee decent, safe, and sanitary housing which is within his financial means and available for immediate occupancy.

At least one relocation specialist is assigned to each highway project to carry out the Relocation Assistance and Payments program. A relocation specialist will contact each person to be relocated to determine individual needs, and to provide information, answer questions, and give help in finding replacement property. Relocation services and payments are provided without regard to race, color, religion, sex, or national origin.

All residential tenants and owner-occupant displacees will receive an explanation regarding all options available to them, such as (1) varying methods of claiming reimbursement for moving expenses; (2) rental replacement housing, either private or publicly subsidized; and (3) purchase of replacement housing.

Financial assistance is available to the eligible residential relocatee to:

- Reimburse the relocatee for the actual reasonable costs of moving from homes acquired for a highway project.
- Provide a supplementary reimbursement payment, when required as per the Uniform Relocation Act, for the cost of a comparable decent, safe and sanitary dwelling available on the private market.
- Provide reimbursement of expenses incidental to the purchase of a replacement dwelling.
- Make payment for eligible increased interest cost resulting from having to get another mortgage at a higher interest rate. Replacement housing payments, increased interest payments, and closing costs are limited to \$31,000 combined total.

A displaced tenant may be eligible to receive a payment, not to exceed \$7,200, to rent a replacement dwelling or room, or to use as a down payment, including closing costs, on the purchase of a replacement dwelling.

Financial assistance is available to the eligible business relocatee to:

- Reimburse the relocatee for the actual reasonable and necessary costs of moving from businesses, and farm operations acquired for a highway project.
- Provide reimbursement for related expenses, such as personal property losses, expenses in locating a replacement site, and certain re-establishment costs.

In certain instances, a business may be eligible for reimbursement as a fixed payment in lieu of the payments for actual moving and related expenses, actual, reasonable re-establishment expenses and search expenses based on the average annual net earnings of their operation (payment may not exceed \$40,000, nor be less than \$1,000). To qualify for this payment the business must vacate or relocate from its displacement site.

The brochures that describe in detail the FDOT's Relocation assistance Program are "Residential Relocation Under the Florida Relocation Assistance Program", "Relocation Assistance Business, Farms and Non-profit Organizations", "Mobile Home Relocation Assistance", and "Relocation Assistance Program Personal Property Moves". All of these brochures are distributed at all public hearings and made available upon request to any interested persons.

14. CONCLUSION

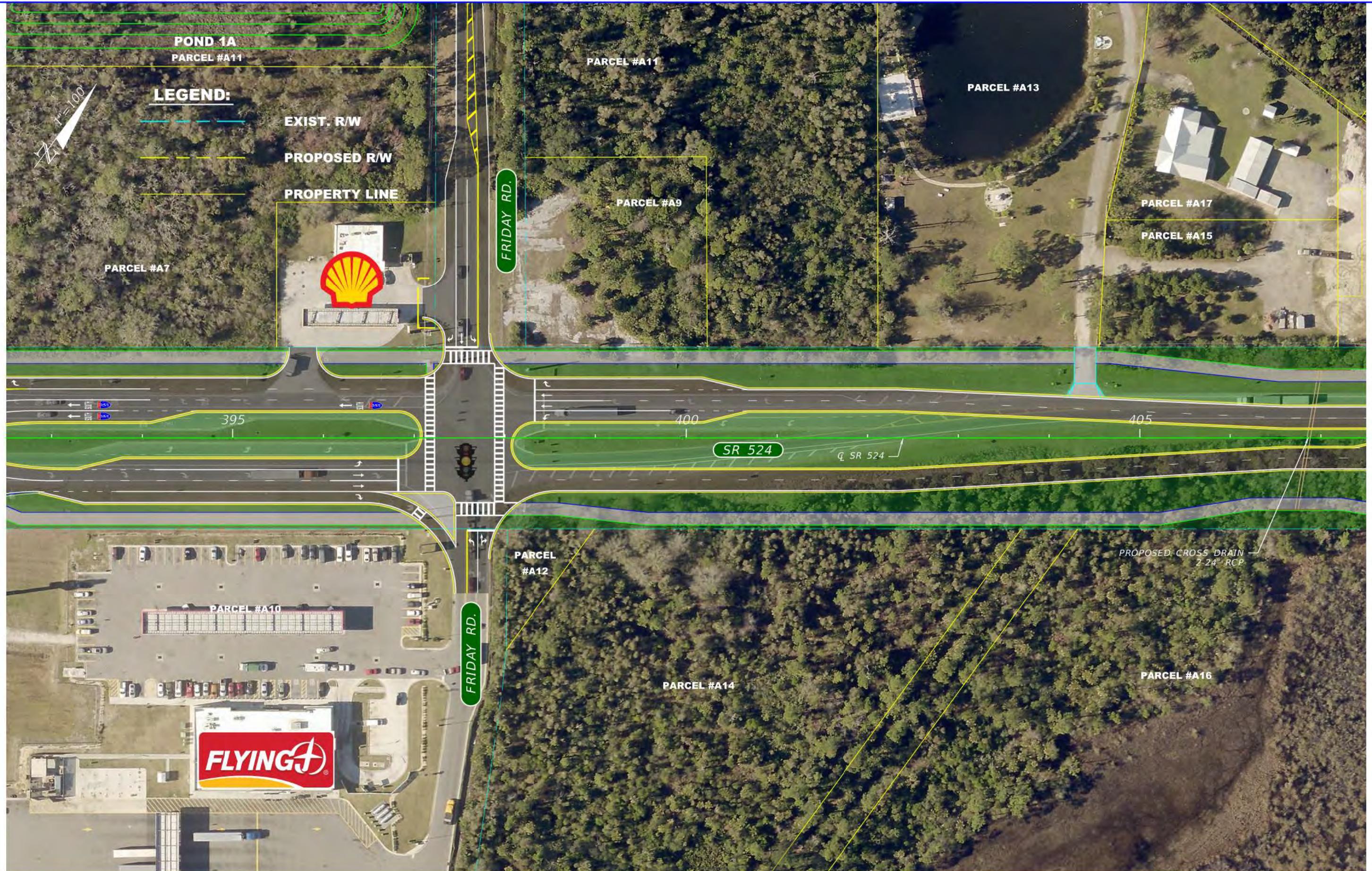
This project will have relocation impacts that will include one business displacement and no residential relocations under the preferred alternative. The business relocatee does not appear to present unusual conditions that would prevent their successful relocation. The research obtained from this study as well as consultations with local government agencies reveals programs to assist business development and relocation. Relocation advisory services and assistance will be provided in accordance with the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended.

APPENDIX A Preferred Alternative Concept Plans

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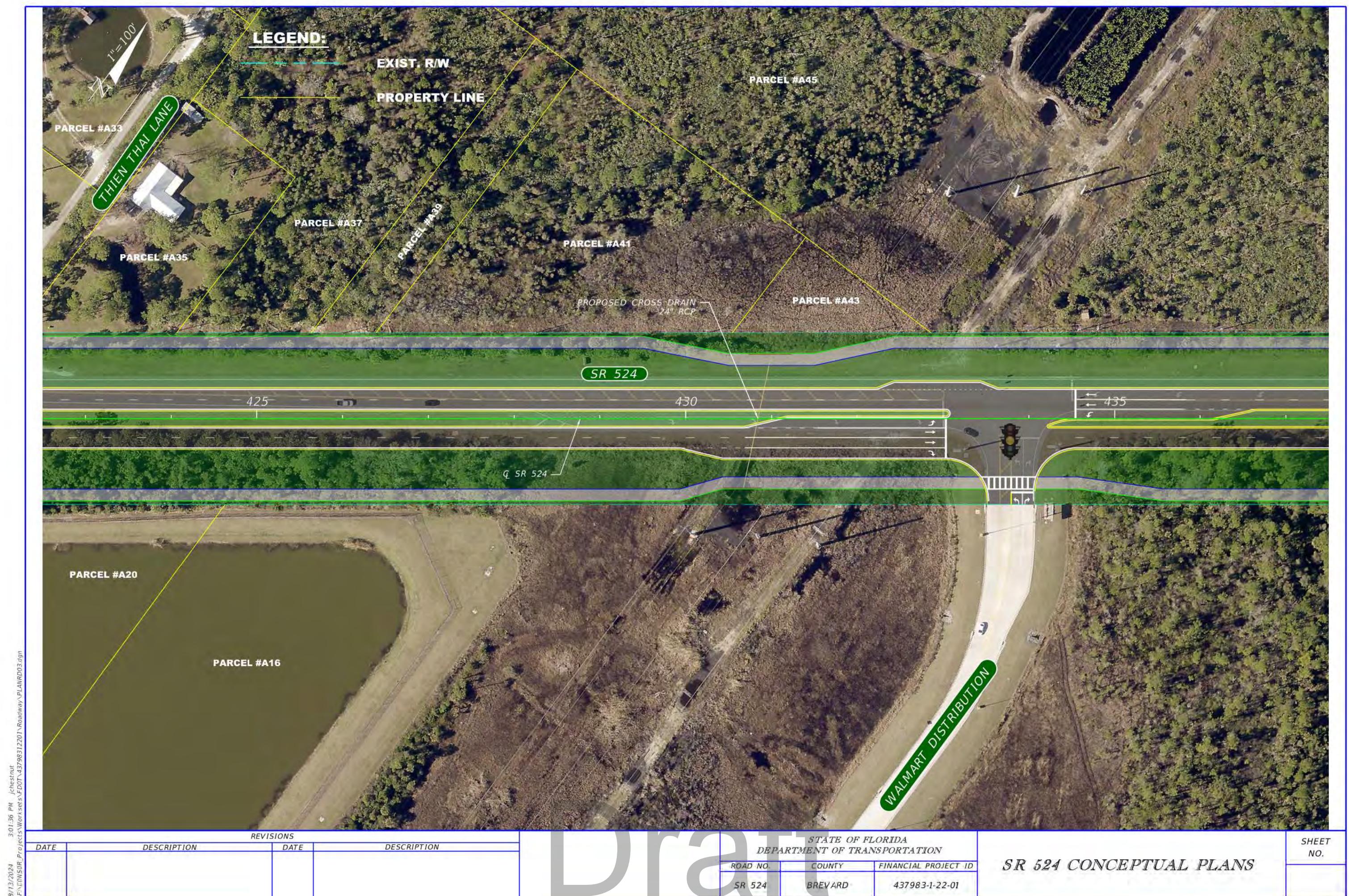
REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 524	BREVARD	437983-1-22-01

SR 524 CONCEPTUAL PLANS

SHEET
NO.





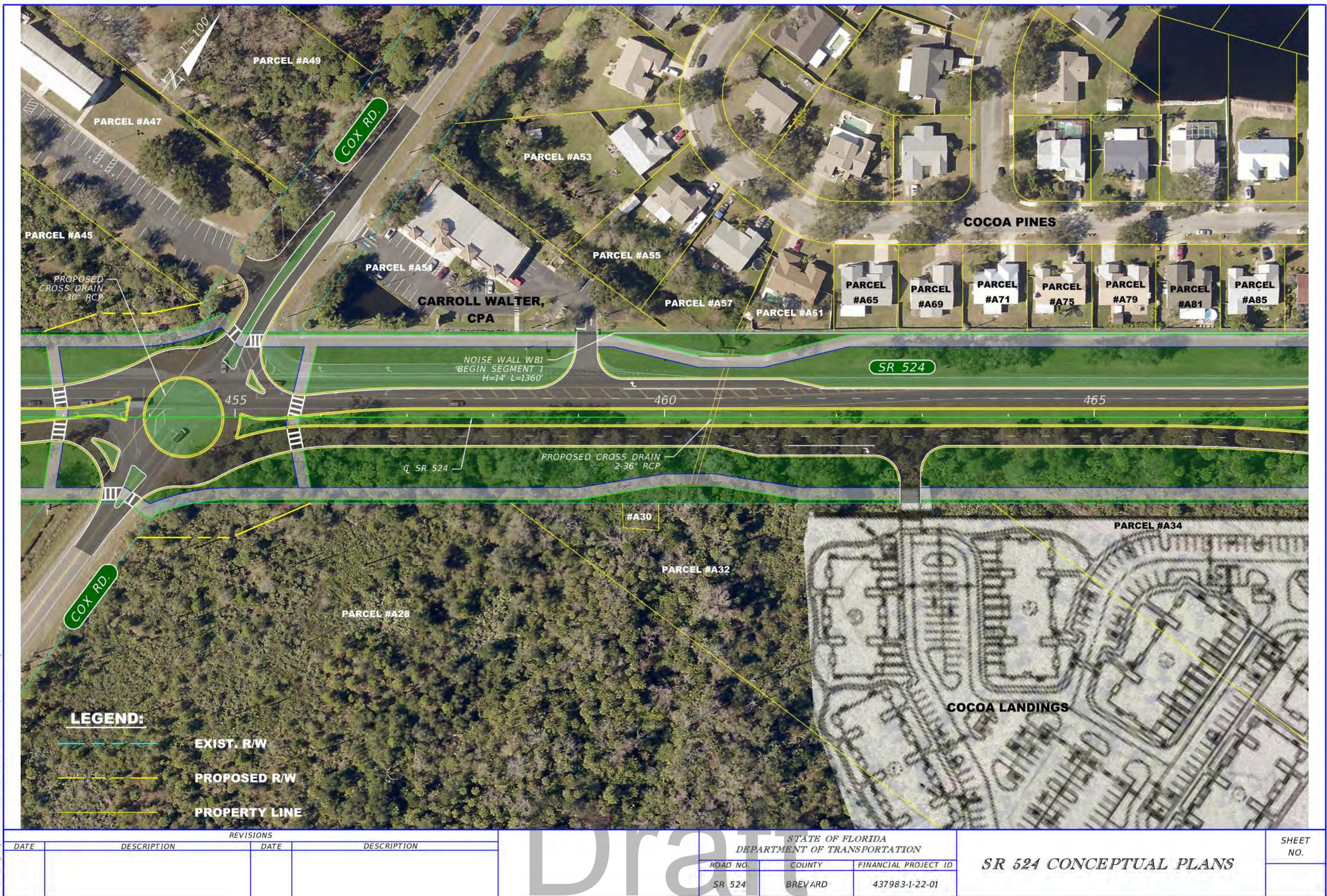


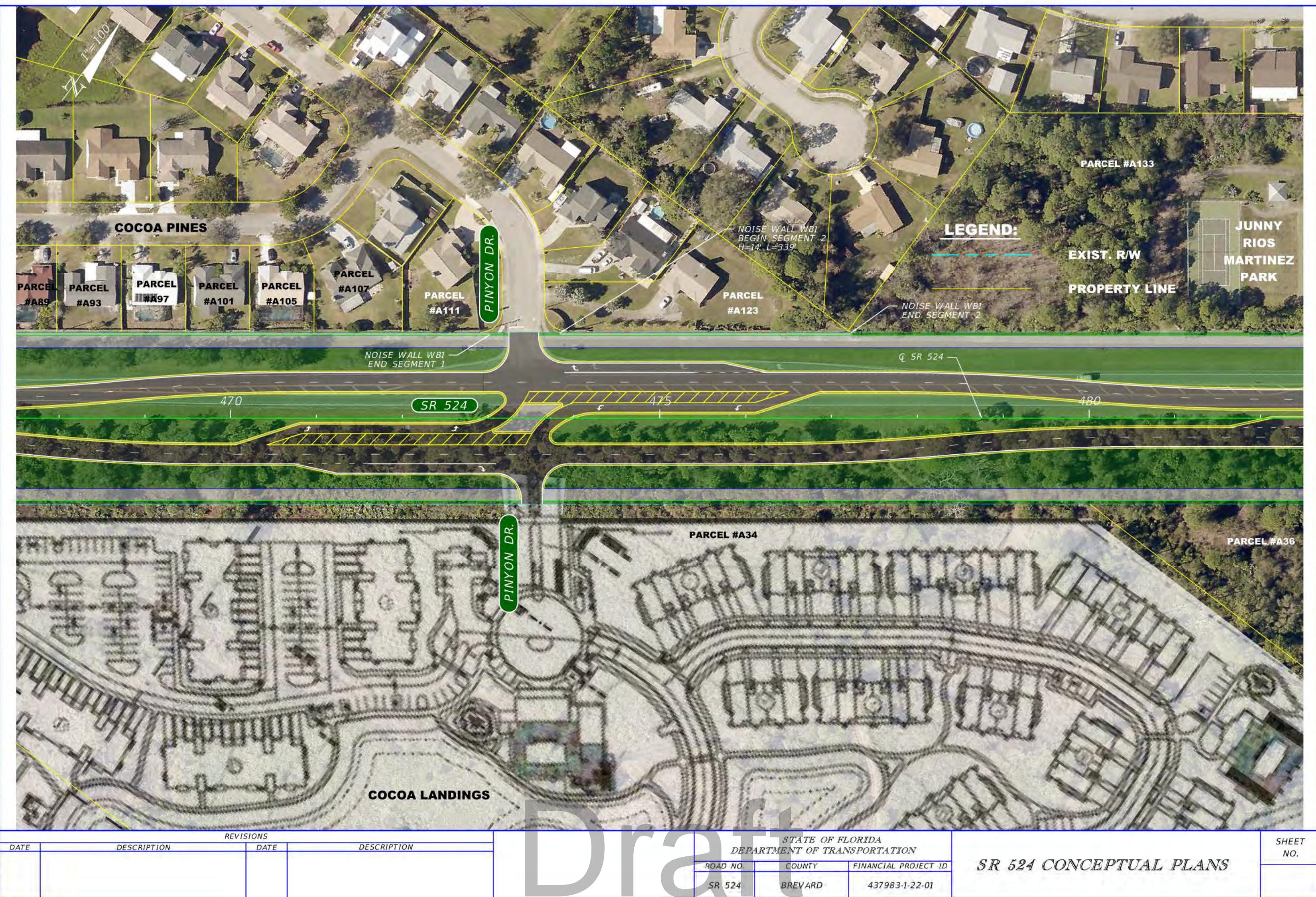
REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

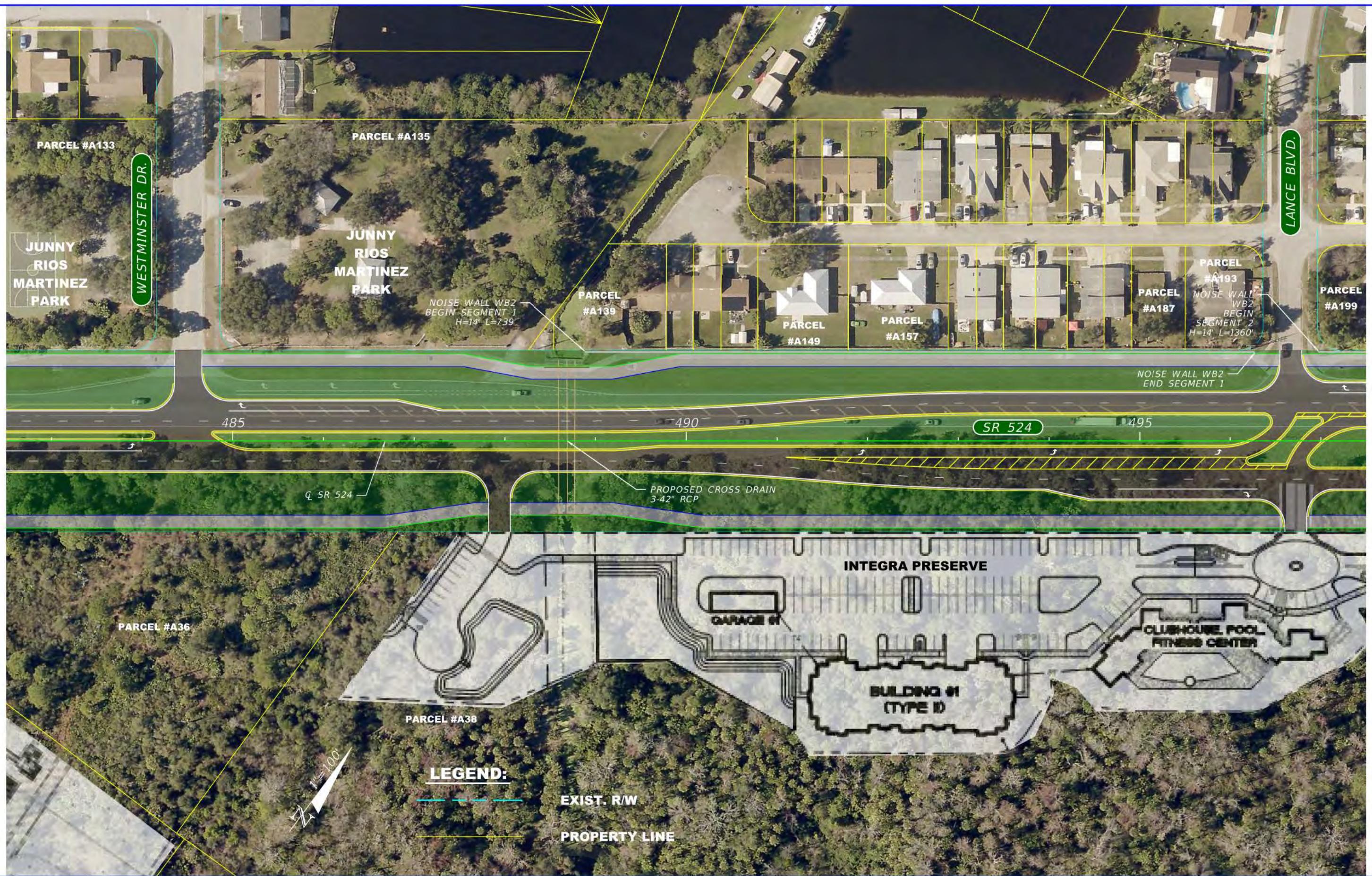
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
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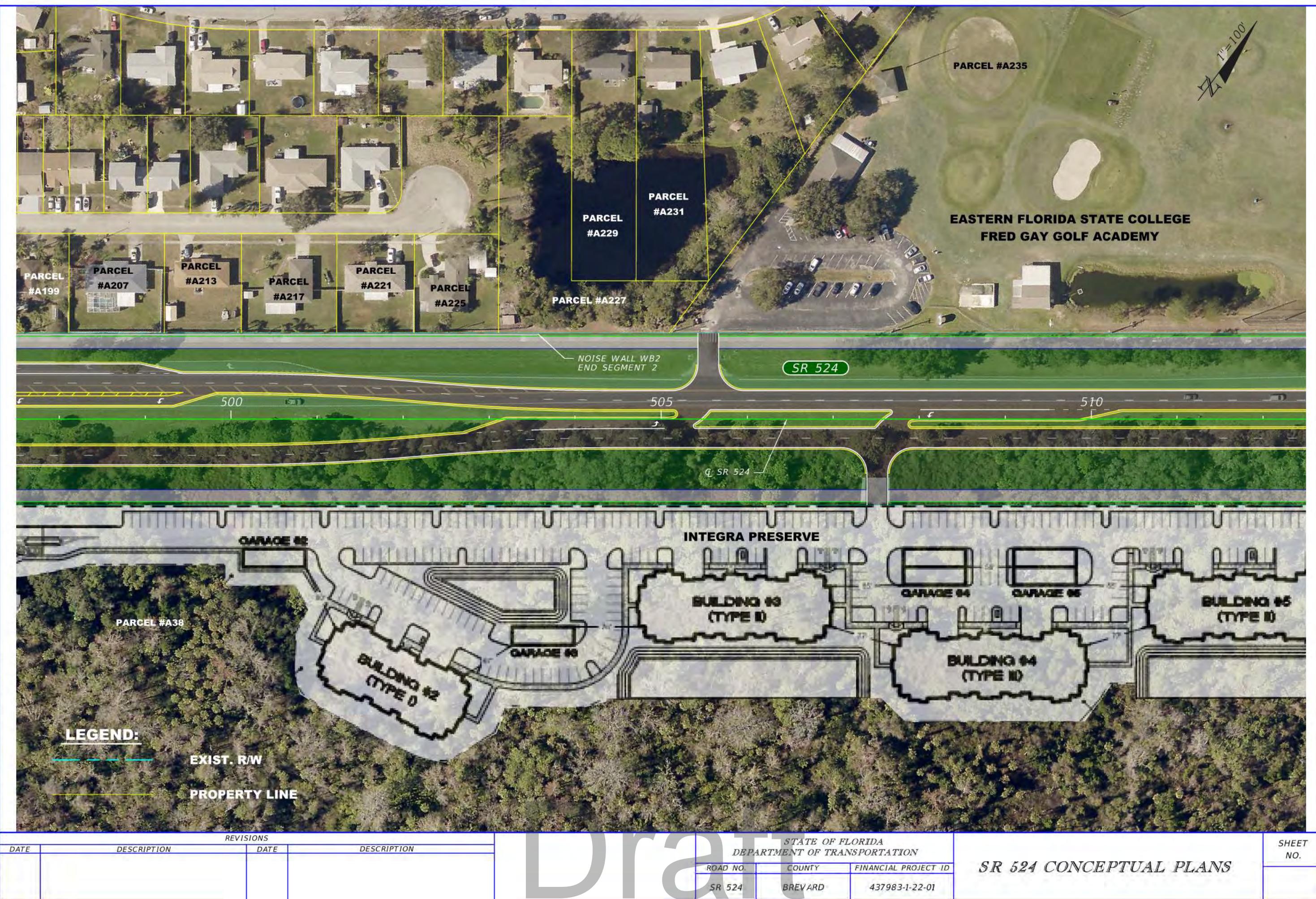






REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

Draft				STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION	SHEET NO.
ROAD NO.	COUNTY	FINANCIAL PROJECT ID	SR 524 CONCEPTUAL PLANS		
SR 524	BREVARD	437983-1-22-01			





REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION





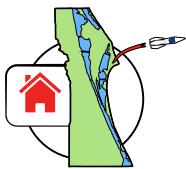
REVISIONS			
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STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			SHEET NO.
ROAD NO.	COUNTY	FINANCIAL PROJECT ID	
SR 524	BREVARD	437983-1-22-01	

SR 524 CONCEPTUAL PLANS

APPENDIX B Brevard County Property Appraiser Property Card

Draft



REAL PROPERTY DETAILS
Account 2454488 - Roll Year 2024

Owners	RM 524 LLC
Mailing Address	402 HIGH POINT DR COCOA FL 32926
Site Address	4301 HIGHWAY 524 COCOA FL 32926
Parcel ID	24-35-23-01-*-1
Taxing District	13D0 - COCOA
Exemptions	NONE
Property Use	1130 - CONVENIENCE STORE WITH GAS PUMP
Total Acres	1.62
Site Code	0309 - STATE ROAD 524
Plat Book/Page	0044/0095
Subdivision	TAYLOR SUBDIVISION
Land Description	TAYLOR SUBDIVISION LOT 1



VALUE SUMMARY

Category	2024	2023	2022
Market Value	\$471,410	\$562,280	\$575,130
Agricultural Land Value	\$0	\$0	\$0
Assessed Value Non-School	\$471,410	\$562,280	\$575,130
Assessed Value School	\$471,410	\$562,280	\$575,130
Homestead Exemption	\$0	\$0	\$0
Additional Homestead	\$0	\$0	\$0
Other Exemptions	\$0	\$0	\$0
Taxable Value Non-School	\$471,410	\$562,280	\$575,130
Taxable Value School	\$471,410	\$562,280	\$575,130

SALES / TRANSFERS

Date	Price	Type	Instrument
09/11/2020	\$500,000	WD	8855/0268
12/06/2012	\$419,600	WD	6752/1541
12/07/2004	\$1,220,000	WD	5403/0360
05/30/1999	\$165,000	WD	4024/3721

BUILDINGS

PROPERTY DATA CARD #1

Building Use: 1130 - CONVENIENCE STORE WITH GAS PUMP

Materials	Details
Exterior Wall:	STUCCO
Frame:	MASNRYCONC
Roof:	BU-TG/MMBRN
Roof Structure:	WOOD TRUSS
Sub-Areas	Extra Features
Base Area (1st)	3,120
Open Porch	80
Total Base Area	3,120
Total Sub Area	3,200
	Paving - Concrete 26,899
	Tank - 12,000 Gallon 1
	Light Poles 6
	Fence - Vinyl 6' 48
	Canopy 3,060
	Tank - 8,000 Gallon 1
	Tank - 6,000 Gallon 1